

Oifig Dearadh Bóthar
Náisiúnta an Mheáin Iarthair
Comhairle Cathrach & Contae Luimnigh

Teach Lios an Fháiltaigh,
Tuar an Daill, Luimneach.
Our Ref: 0107_07_00006



Mid West
National Road Design Office
Limerick City & County Council

Lissanalta House, Dooradoyle Road,
Dooradoyle, Limerick.
27th May 2016

**TO/ THE CATHAOIRLEACH & EACH MEMBER OF THE MUNICIPAL DISTRICT
OF ADARE-RATHKEALE**

**RE: PART 8 DEVELOPMENT – N21 ADARE WESTERN APPROACH
IMPROVEMENT SCHEME**

I enclose, herewith, signed report prepared in accordance with Section 179 of the Planning & Development Acts 2000-2015 and Part 8 of the Planning & Development Regulations 2001-2015 in respect of the above proposed development.

The plans and particulars went on public display from Monday, 14th March 2016 to Wednesday, 27th April 2016. Submissions were received, details of which are set out in the attached Part 8 Planning Report.

It is proposed to proceed as indicated in the Planning Report.

Signed: _____


MARK LEAHY
ADMINISTRATIVE OFFICER

Encl.



Telephone 061 - 496800

Fax 061 - 583150

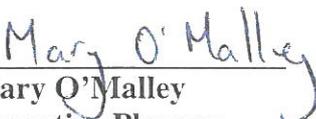
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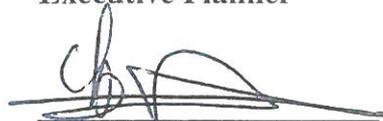
**PLANNING REPORT IN ACCORDANCE WITH
SECTION 179 3(a) OF THE PLANNING AND DEVELOPMENT ACT 2000 – 2015**

Re: Proposed N21 Adare Western Approach Improvement Scheme works including resurfacing of the N21 from the L1422 Blackabbey Road to the R519 Ballingarry Road, reconstruction of sections of failed pavement, the provision of drainage systems with attenuated discharge and hydrocarbon interception, the provision of new and upgraded kerbing and footpaths, an off-road public car park opposite Scoil Naomh Iosaf and all ancillary works

At: Adare, Graigue & Cummeen

Planning Reference no. 16-8001


Mary O'Malley
Executive Planner


Stéphane Duclot
A/Senior Planner


Donogh O'Donoghue
A/Senior Executive Planner


Caroline Curley
Director of Services
Regional Services Directorate

Pursuant to Section 179 3(a) of the Planning & Development Act 2000-2015, this report is submitted to the members of Limerick City & County Council. In accordance with Section 179(4) (b) of the above Act, it is proposed to proceed as indicated in Section 5 of this report


Pat Dowling
Deputy Chief Executive
Limerick City and County Council

Date: 26 May 2016

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1.0 - Foreword

This planning report has been prepared pursuant to Section 179 of the Planning and Development Act 2000-2015, and Part 8 of the Planning and Development Regulations 2001-2015.

2.0 - Description of the nature and extent of the proposed development

The proposal is to improve the road pavement and drainage in the N21 western approach to Adare village. The proposed works include:

- Improving the quality of footpaths and the provision of new footpaths to ensure connectivity for pedestrians. Where sufficient width permits, footpaths will be constructed along the carriageway in front of residential properties.
- Provision of a car park facility to provide a safer drop off area for Scoil Naomh Iosaif Primary School as well as to reduce congestion during school drop off and collection times on the N21.
- The scheme includes for the construction of drainage in the western approach to the village of Adare. At present there are low spots on the carriageway which are prone to ponding during periods of prolonged rainfall. The redesign of the pavement as well as a new drainage system will curtail ponding of surface water. Furthermore the construction of an attenuation tank will ensure controlled discharge of surface water runoff into the river.

The plans and particulars went on public display from 14th of March 2016 to 27th April 2016. Submissions and observations had to be submitted by 12th of May, 2016.

3.0 - Likely implications, if any with respect to the proper planning and sustainable development of the area

It is considered that the proposed improvements will have a positive effect in the area, the proposed car park facility would provide a safer drop off area for Scoil Naomh Iosaf Primary School as well as to reduce congestion during school drop-off and collection times on the N21. The footpath improvement proposals would provide greater connectivity for pedestrians and also a safer environment and the proposed new drainage system would curtail ponding of surface water. The proposed new attenuation tank at the car park location would ensure controlled discharge of surface water runoff into the river. It is considered that no implications arise on the proper planning and sustainable development of the area.

3.1 Relevant Policies and Objectives

The Limerick County Development Plan 2010-2016 sets out the following:

Policy IN P7: Road Safety and Capacity

To seek the improvement of road safety and capacity throughout the County, through minimising existing traffic hazards, preventing the creation of additional or new traffic hazards in the road network and securing appropriate signage.

Objective IN O24: Protection of Surface water bodies

It is the objective of the Council to ensure the integrity of surface water bodies is maintained; and where damaged, to seek, as resources allow, to restore their integrity. Priority will be given to those waters deemed to be sensitive in respect of their uses, and vulnerable due to low assimilation capacity. The Council shall give particular priority to the need to protect human health, designated habitats, and to minimise costs of water/wastewater treatment.

The site is within the development boundary of the Adare Local Area Plan (LAP) 2012-2018. The LAP sets out the following:

Policy T1: It is the policy of the Council to improve accessibility; reduce dependence on private car transport and encourage the use of energy efficient forms of transport and alternatives to the private car.

Policy T2: It is the policy of the Council to ensure that all proposals shall comply with the policies, objectives and development management standards of the Limerick County Development Plan, 2010 – 2016 in relation to transport and infrastructure and the objectives outlined below.

Objective T5: Car parking and traffic management

It is an objective of the Council to encourage the provision of off-street public parking areas as part of any application for development.

Objective T6: Safeguard the Capacity of the N21

It is the objective of the Council to safeguard the capacity of the N21 to ensure that any future developments do not compromise the strategic function of these roads.

3.2 Habitats Directive Project Screening Assessment

Habitats Directive Project Screening Assessment for Appropriate Assessment

<p>Construction Phase: resurfacing of the N21 from the L1422 Blackabbey Road to the R519 Ballingarry Road, reconstruction of sections of failed pavement, the provision of drainage systems with attenuated discharge and hydrocarbon interception, the provision of new and upgraded kerbing and footpaths, an off-road public car park</p> <p>Are effects significant: No</p> <p>Are substantial works required: Yes</p> <p>Operating phase effects: Limited and localised</p> <p>Are effects significant: No</p>	<p>Ex-situ effects: n/a</p> <p>Are effects significant No</p> <p>Run-off: No</p> <p>Are effects significant: No</p> <p>Abstraction: None</p> <p>Displacement: None</p>
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Identification of Natura 2000 sites which may be impacted by the proposed development

1	Impacts on designated rivers, streams, lakes and fresh water dependant habitats and species e.g. bogs or otters -see abstraction/run off etc above.	<i>Is the development in the relevant catchment of or immediately up/downstream of a watercourse that has been designated as a Natura 2000 site?</i>	No
2	Impacts on terrestrial habitats and species.-see area and disturbance /displacement effects above.	<i>Is the development within 1km of a SAC site with terrestrial based habitats or species?</i> Name of site: Lower River Shannon Sac site	540m
3	Impacts on designated marine habitats and species.	<i>Is the development located within marine or intertidal areas or within 5 km of an SAC site whose qualifying habitats or species include the following habitats: Salmonid, Lamprey Mudflats, Sandflats, Saltmarsh, Shingle, Reefs, Sea Cliffs</i> Name of site: Lower River Shannon Sac site	540m

4	Impacts on birds in SPAs-	<i>Is the development within 1km of a Special Protection Area</i>	No
5	Cumulative effects	Would consideration of a number of significant projects nearby such as forested areas, quarries, wind energy together with the proposed development significantly increase the impacts listed above:	Refer to below.

Conclusion

Overall it is considered that the development as proposed should not exercise a significant effect on the conservation status of any SAC and SPA. The applicants have conducted a Screening for Appropriate Assessment of Natura 2000. The information has been assessed by the Heritage Officer and the proposal is considered acceptable. On the basis of the findings it is considered that the proposed development will not have a significant effect on the Natura 2000 sites.

4.0 - Submissions with respect to the proposed development

LIST OF SUBMISSIONS RECEIVED		
SUB REF	FROM	Date received
(A)	Waste Management Section, Environment, Limerick City & County Council	19/04/2016
(B)	Keenan Property Management c/o Catherine Dunford, 14 Penrose Wharf, Cork	29/03/2016
(C)	Fewer Harrington & Partners c/o Shane Lahiff, Studio 14, The Atrium, Maritana Gate, Canada Street, Waterford	23/03/2016
(D)	Adare Tidy Towns, Adare, Co. Limerick	02/05/2016
(E)	Residents of Adare via Cllr Adam Teskey	13/05/2016
(F)	Stephen Lavin, Scoil Naomh Íosaf, Adare	10/05/2016
(G)	Karina Byrne, Parents Council, Scoil Naomh Íosaf, Adare	10/05/2016
(H)	Sean Collins, Sean Collins Bar, Adare	12/05/2016
(I)	Adare Community Council	12/05/2016
(J)	Tony Sexton, Ardshanbally, Adare, Co. Limerick	12/05/2016
(K)	Department of Arts Heritage and the Gaeltacht	11/05/2016
(L)	Gas Networks Ireland	21/03/2016
(M)	County and City Archaeologist	26/05/2016

SUB (A) Waste Management Section

Submission description:

- Waste Management Plan required

Planning Departments Comments

- Acknowledgement of submission. An appropriate condition will be attached in relation to the provision of a Waste Management Plan for the proposed works.

SUB (B) Keenan Property Management

Submission description:

Ms. Catherine Dunford working on behalf of Keenan Property Management has requested Limerick City & County Council to investigate the possibility of constructing a yellow box at the signalised junction of the Village at Adare Manor and the N21. She noted in her submission that when traffic backs up on the N21 it often blocks this junction and prevents traffic from exiting The Village or Manor Fields.

Mid West National Road Design Office Comments:

The proposals have been amended to include for a yellow box at this junction.

SUB (C) Fewer Harrington & Partners

Submission description:

Shane Lahiff, on behalf of Fewer Harrington & Partners, requested Limerick City & County Council to investigate the proposal put forward by Fewer Harrington & Partners regarding a joint access road off the N21 into the proposed car park which would also serve the proposed retirement village being promoted by FH&P.

Mid West National Road Design Office Comments:

The matter of access to these lands must be subject to a separate planning application and discussions between Limerick City & County Councils Property Section.

SUB (D) Adare Tidy Towns

Submission description:

Adare Tidy Towns, in their submission, requested that as much construction work as possible should be conducted at night to minimize the disruption to traffic flow.

Mid West National Road Design Office Comments:

Limerick City & County Council will provide for certain construction activities relating to the main construction works to be carried out at night in order to minimize the disruption to traffic flow.

SUB (E) Residents of Adare

Submission description:

Adare residents via Cllr. Teskey have requested that Limerick City & County Council consider the proposal to construct a yellow box at the N21 / Blackabbey Road junction. At present motorists cannot join the N21 from the Blackabbey Road during periods of congestion.

Mid West National Road Design Office Comments:

The proposals have been amended to include for a yellow box at this junction.

SUB (F) Stephen Lavin – Scoil Naomh Íosaf

Submission description:

Mr. Lavin (Principal of Scoil Naomh Íosaf) welcomed the proposed works on the N21. He noted in his submission that the proposed car park across from the school and the widened footpaths are huge improvements to health and safety outside the school. Mr. Lavin requested that a vehicle actuated sign should be placed adjacent to the “SLOW” road marking outside the school.

Mid West National Road Design Office Comments:

The proposals have been amended to include for a vehicle actuated speed sign.

SUB (G) Parents Council, Scoil Naomh Íosaf,

Submission description:

Ms. Byrne is Chairperson of Scoil Naomh Íosaf Parent’s Council. In her submission she noted that the parent’s council support the proposed changes to the N21. She also noted that health and safety would be improved with the addition of parking spaces.

Mid West National Road Design Office Comments:

Submission is noted

SUB (H) Sean Collins

Submission description:

Mr. Collins requested a “Build out” in front of his premises to ensure sightlines are not impeded by parked vehicles when exiting the laneway adjacent to his property. Mr. Collins also requested that the car park be open all year.

Mid West National Road Design Office Comments:

A build out in front of Sean Collins Bar will be considered as part of the detailed design. The car park will be available to the school and for business and public parking.

SUB (I) Adare Community Council

Submission description:

In their submission, the Adare Community Council welcome the proposed works on the N21. The Adare Community Council requested diversion routes be signposted at Newcastle West, Limerick City, the M7, M18 and N20.

Adare Community Council also requested that the Foul Sewer be extended from the Village Hall to the Traffic Lights to accommodate all houses on the Rathkeale Road to Murphy's Cross, plus any future development on the southern/Adare Manor side of the traffic lights. The Foul Sewer also needs to be extended west beyond the lights to cater for future development on the council land at the Deerpark side. Ducting for all other services required for those developments should be included.

Mid West National Road Design Office Comments:

Limerick City & County Council will signpost alternative routes during the construction stage for the scheme. Variable Message Signs will be provided at suitable locations in advance and during the course of construction for the main works contract. The purpose of these Variable Message Signs will be to provide advance notice to commuters from areas including Newcastlewest, Rathkeale, Limerick City / M20 / N69 to use an alternative route in order to avoid delays due to roadworks.

The provision of a Foul Sewer is a function of Irish Water. Limerick City & County Council have brought this submission to the attention of Irish Water. Extensive consultations have taken place between Irish Water and Limerick City & County Council with regard to the provision of a Foul Sewer.

The Council are of the view that any service, foul sewer or otherwise, planned for this section of the N21 should be installed as part of these road improvements and as such have, and will continue to give, favourable consideration to the inclusion of the foul sewer in the works.

Limerick City & County Council will consider the inclusion of a Foul Sewer along the N21 from Blackabbey Road as far as the Council lands located approximately 120m south-west of the Traffic Lights into the main contract for this scheme subject to the condition that all costs (including costs associated with the risks of constructing the Sewer) will be provided by Irish Water.

It is our understanding from discussions with Irish Water that a Foul Sewer from the Traffic Lights at the Village to Murphy's Cross could be provided in the future through the wayleave located behind the houses fronting the N21 between the Traffic Lights at the Village and Murphy's Cross.

Limerick City & County Council will provide ducting to accommodate future services. Service Providers have been advised that services installed post construction will be subject to the provisions of Section 4.5.2 of the Guidelines for Managing Openings in Public Roads (Second Edition, September 2015). This requires that any services laid during a specified protected period will be subject to onerous reinstatement conditions (full width/lane/bay reinstatement etc.) as the Road Authority intends to ensure that such works do not compromise the structural integrity and design life of the new road pavements and footpaths.

SUB (J)

Tony Sexton

Submission description:

Mr Sexton welcomed the scheme in his submission. He made reference to the footpath extension and safety improvements to Scoil Naomh Íosaf. He noted the scheme should be future proofed including provisions for foul sewer pipes, gas pipes and IT service ducting.

Mid West National Road Design Office Comments:

The provision of a Foul Sewer is a function of Irish Water. Limerick City & County Council have brought this submission to the attention of Irish Water. Extensive consultations have taken place between Irish Water and Limerick City & County Council with regard to the provision of a Foul Sewer.

The Council are of the view that any service, foul sewer or otherwise, planned for this section of the N21 should be installed as part of these road improvements and as such have, and will continue to give, favourable consideration to the inclusion of the foul sewer in the works.

Limerick City & County Council will consider the inclusion of a Foul Sewer along the N21 from Blackabbey Road as far as the Council lands located approximately 120m south-west of the Traffic Lights into the main contract for this scheme subject to the condition that all costs (including costs associated with the risks of constructing the Sewer) will be provided by Irish Water.

Limerick City & County Council will provide ducting to accommodate future services. Service Providers have been advised that services installed post construction will be subject to the provisions of Section 4.5.2 of the Guidelines for Managing Openings in Public Roads (Second Edition, September 2015). This requires that any services laid during a specified protected period will be subject to onerous reinstatement conditions (full width/lane/bay reinstatement etc.) as the Road Authority intends to ensure that such works do not compromise the structural integrity and design life of the new road pavements and footpaths.

SUB (K)

Department of Arts Heritage and the Gaeltacht

Submission description:

While not a legal requirement, it would be best practice to minimise or avoid as far as possible the removal or disturbance of hedgerows during the bird nesting season (March to August inclusive).

Mid West National Road Design Office Comments:

It is intended to construct the main construction works for the project outside the tourist season, which broadly coincides with the March-August window. The removal or disturbance of hedgerows, where practicable, will be undertaken outside the period requested.

SUB (L)

Gas Networks Ireland

Submission description:

Gas Networks Ireland (GNI) submitted drawings pertaining to the existing gas network infrastructure within the scheme extents. The information is provided as a general guide and GNI noted in their submission that they cannot guarantee its accuracy and it should not be relied upon for accurate distance or depth of cover.

Mid West National Road Design Office Comments:

The information provided in this submission will be included in the existing services location drawings which will be issued to all tenderers during the tender process.

SUB (M) County and City Archaeologist

Submission description:

- Archaeological conditions required.

Planning Departments Comments

- Acknowledgement of submission. Appropriate conditions will be attached in relation to archaeological testing and monitoring.

5.0 - Action taken by Local Authority

It is proposed to proceed with the development in accordance with the drawings and the details and specifications contained in this report.

1. The development shall be carried out in accordance with the plans and particulars lodged with the application, as amended by the further plans and particulars submitted on the 14th day of March, 2016 and on the 17th day of May, 2016 and, except as may otherwise be required in order to comply with the following conditions.
Reason - In order to clarify the development to which this permission applies.
2. Prior to the commencement of development the applicant shall submit to the Planning Authority for agreement in writing a waste management plan for the recovery/disposal of all wastes arising from the demolition and construction works and all other associated works.
Reason - In the interest of the proper planning and sustainable development of the area.
3. Due to the location of the potential line of town defences lying within the scheme, advance testing at this location shall occur to establish if there are any features associated with the town defences evident. In addition the site of the proposed car-park shall also be archaeologically tested in advance of the scheme. To this end the services of a suitably qualified licensed archaeologist shall be engaged. No sub-surface work shall be undertaken in the absence of the archaeologist without his/her express consent. This includes all other site investigations and tests. Archaeological test trenches shall be excavated at the designated locations by the archaeologist (licensed under the National Monuments Acts 1930-2004). Having completed the

work, the archaeologist shall submit a written report to the Planning Authority and to the Heritage and Planning Division of the Department of Arts, Heritage and the Gaeltacht in advance of the commencement of construction.

Reason – In order to conserve the archaeological heritage of the area and to secure the preservation of any remains which may exist within the site.

4. Archaeological monitoring by a qualified archaeologist shall be in place for all ground disturbance associated with the development. The name of the archaeologist shall be submitted to the Planning Authority **four** weeks in advance of the commencement of any site works. Should archaeological material be found during the course of monitoring, the archaeologist may have work on the site stopped, pending a decision as to how best to deal with the archaeology. The Department of the Arts, Heritage and the Gaeltacht and the Planning Authority Archaeologist shall be informed. The developer shall be prepared to be advised by the Department of the Arts Heritage and the Gaeltacht with regard to any necessary mitigating action and shall facilitate the archaeologist in recording any material found. On completion, an archaeological report detailing the works shall be submitted to the Planning Authority and the Department of the Arts, Heritage and the Gaeltacht.

Reason – In order to conserve the archaeological heritage of the area and to secure the preservation of any remains which may exist within the site.



Part 8 Road's Report

N21 Adare Western Approach

Report Number: 0107/07/00005

May 2016



Mid West National Road Design Office
Lissanalta House,
Dooradoyle
County Limerick

Tel: 061 496 800

Fax: 061 583 150

e-mail: info@midwestroads.ie



Mid West
National Road Design Office

Document Control Sheet

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Appendix 1 Part 8 Particulars *including:*

Appendix A – Part 8 Notice & Drawings

Appendix B – Appropriate Assessment Screening Report & Cultural Heritage Assessment

Appendix 2 Submissions Received

Appendix 3 Site Layout Plans

1 INTRODUCTION

1.1 GENERAL

In accordance with Part VIII of the Planning and Development Regulations 2001 to 2015, Limerick City and County Council made available for public inspection, plans and particulars of the proposed scheme between, and including, the dates of 14th March 2016 and 27th April 2016.

This report describes the particulars of the proposed works, lists the submissions received further to the public consultation process and presents the responses of Limerick City and County Council to these submissions.

The particulars of the Scheme were described in the report particulars to accompany Part 8 Planning Drawings for N21 Adare Western Approach Improvement Scheme (Report No. 15050-01-019), which was made available for public inspection. Refer to Appendix 1 for Part 8 Particulars and Drawings.

1.2 SCHEME LOCATION

The location of the Scheme is shown in Figure 1: Site Location Map below.

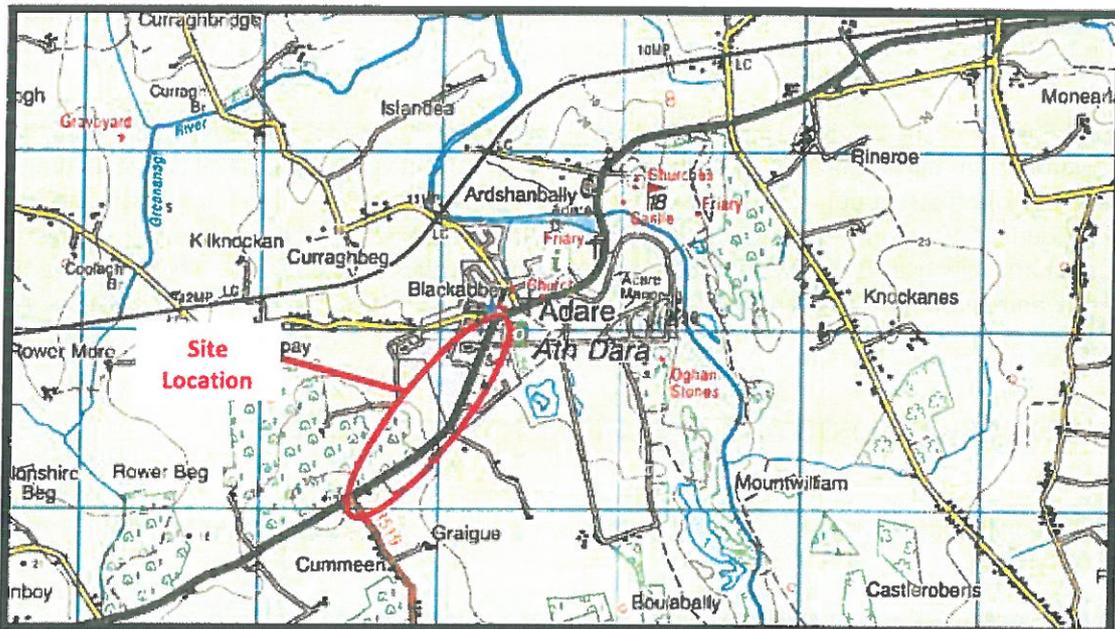


Figure 1: Site Location Map

2 NEED FOR THE SCHEME

2.1 GENERAL

The Scheme is located on the N21 on the western approach to Adare village. The N21 National Primary road at the scheme location is an urban road fronted by both commercial premises and

residential properties along its length. The intensity of development increases as one travels towards the village centre. The Scheme is located in the townlands of Adare, Graigue and Cummeen.

Transport Infrastructure Ireland identified the Scheme as a Pavement Strengthening Scheme as there is evidence of much deterioration to the existing pavement and road lining on the western approach to the village. It was noted during the preliminary site visit that footpaths are in poor condition, do not cater for all pedestrian desire lines and at times only serve one side of the carriageway. Drainage systems in place are inadequate to cater for surface water runoff and need to be upgraded over the entire Scheme. The upgraded drainage along the scheme will improve the lifespan of the pavement.

To enhance safety, particularly in the vicinity of Scoil Naomh Íosaf primary school adjacent to the N21, a lay-by has been designed to ensure parents and school children can open their car doors safely when entering / exiting a vehicle on the N21.

2.2 COUNTY DEVELOPMENT PLAN

The Adare Local Area Plan 2015 – 2021 sets out Limerick City and County Council planning policies for the village of Adare. The plan highlights that the N21 National Primary Road is the main tourist and traffic route serving the southwest of Ireland.

In section 6.1 of the LAP it is noted that on street parking on both sides of the N21 can restrict traffic movements, in particular trucks passing each other. An objective of the Council is to encourage the provision of off street public parking areas as part of any application for development. Limerick City and County Council in this Part 8 application have taken cognisance of the Adare LAP addressing in some part objective T3 (b) by completion of the gap in the footpath on the N21 between the Traffic Lights and Murphy's Cross and objective T5 by the provision of a car park opposite Scoil Naomh Íosaf.

3 OBJECTIVES FOR AND DESCRIPTION OF THE SCHEME

3.1 SCHEME OBJECTIVES

The purpose of the Scheme is to improve the road pavement and drainage on the N21 western approach to Adare village. The scheme objectives also include improving the quality of footpaths and the provision of new footpaths to ensure connectivity for pedestrians. The Scheme includes the provision of a car park facility to provide a safer drop off area for the school as well as reduce congestion during school drop off and collection times on the N21.

3.2 GENERAL DESCRIPTION

The proposed Scheme consists of works to improve pavement quality, aid traffic calming and improve road safety.

Where sufficient width permits, footpaths will be constructed along the carriageway in front of residential properties which will provide greater connectivity for pedestrians and also a safer environment. At present parking in Adare is limited. Traffic flow is often congested in the vicinity of the school as a result of double parking during school drop off and pick up times. In response to these difficulties, Limerick City and County Council have included for the provision of a car park opposite Scoil Naomh Íosaif National School which will serve all businesses in the immediate vicinity as well as the National School.

The scheme also includes for the construction of drainage on the western approach to the village of Adare. At present there are low spots on the carriageway which are prone to ponding during periods of prolonged rainfall. The redesign of the pavement as well as a new drainage system will curtail ponding of surface water. Furthermore, the construction of an attenuation tank will ensure controlled discharge of surface water runoff into the river.

4 DESIGN STANDARDS

The proposed Scheme was designed in accordance with the following documents:

- NRA Design Manual for Roads and Bridges
- Traffic Signs Manual (Chapter 8) – Department of Transport – November 2010

5 LAND ACQUISITION

Limerick City and County Council has sought to minimize the impact of the proposed Scheme on local residents and property owners, in so far as is reasonably practicable. Land acquisition is required from Scoil Naomh Íosaif for works in the vicinity of the parking layby. Engagement with the land owner has been ongoing from the Scheme commencement.

6 ENVIRONMENTAL ASSESSMENT

6.1 APPROPRIATE ASSESSMENT SCREENING REPORT

Appropriate Assessment Screening of the proposed scheme was carried out by Limerick City & County Council in accordance with Article 6(3) of the EU Habitats Directive (92/43/EEC).

The assessment identified two Natura 2000 sites located close to the proposed scheme:

- the Mague River – Lower River Shannon SAC site (No. 002165), located 500m from the scheme;
- the Adare Woodlands Proposed Natural Heritage Area, located beside the proposed scheme.

The assessment notes that:

- the scheme does not encroach onto either site;
- the proposed works are largely contained within the footprint of the existing road;
- surface water discharge will be attenuated and hydrocarbon interception will be used.

The assessment concludes that the impacts of the proposed works on ecology and hydrology are not considered to be significant. It concludes that full Appropriate Assessment is not required as the effects of the works on the Natura 2000 sites are not judged to be significant. Recommendations on mitigation measures emerged from the Assessment. The measures relate generally to the construction stage of the scheme and will be incorporated into the design and specification of the works. The Appropriate Assessment Screening Report is included in Appendix 1.

6.2 CULTURAL HERITAGE ASSESSMENT REPORT

A Cultural Heritage Assessment was undertaken to investigate the potential impact of the scheme on the Archaeological and Architectural Heritage. The results and conclusions of this assessment are outlined in the Cultural Heritage Assessment Report included in Appendix 1.

7 SUBMISSIONS RECEIVED TO PART 8 PLANNING

7.1 SUBMISSIONS RECEIVED

Table 2: Submissions Received, below provides a schedule of the submissions received during the public consultation process. A copy of each submission is included in Appendix 2.

Submission Ref.	Submitted By
No. 1	Keenan Property Management c/o Catherine Dunford, 14 Penrose Wharf, Cork
No. 2	Fewer Harrington & Partners c/o Shane Lahiff, Studio 14, The Atrium, Maritana Gate, Canada Street, Waterford
No. 3	Adare Tidy Towns, Adare, Co. Limerick
No. 4	Residents of Adare via Cllr Adam Teskey
No. 5	Stephen Lavin, Scoil Naomh Íosaf, Adare
No. 6	Karina Byrne, Parents Council, Scoil Naomh Íosaf, Adare
No. 7	Sean Collins, Sean Collins Bar, Adare
No. 8	Adare Community Council
No. 9	Tony Sexton, Ardshanbally, Adare, Co. Limerick

Table 1: Submissions Received

7.2 PRESCRIBED BODIES / EXTERNAL CONSULTATIONS

Table 3 below provides a Schedule of the Prescribed Bodies/External Consultations consulted, and responses received during the consultation process.

Prescribed Bodies/External Consultations	Registered Letter No.	Response
An Chomhairle Ealaíon The Arts Council 70 Merrion Square Dublin 2	RL 5395 4395 6IE	No response
The Heritage Council Aras na hOidhreacht Church Lane Kilkenny	RL 5395 4394 2IE	No response
An Taisce Tailor's Hall Back Lane Dublin 8	RL 5395 4393 9IE	No response
Bus Éireann South West Regional Manager Roxboro Road Limerick	RL 5395 4392 5IE	No response
Chief Superintendent An Garda Síochána Newcastle West Co Limerick	RL 5395 4391 1IE	No response
Development Applications Unit Dept of Arts, Heritage & Gaeltacht Newtown Road Wexford	RL 5395 4390 8IE	Response received from Yvonne Nolan, Development Applications Unit
Transport Infrastructure Ireland, Parkgate Business Centre, Parkgate Street Dublin 8	RL 5395 4389 9IE	Acknowledged receipt of letter – No comment
Gas Networks Ireland P.O. Box 51, Gasworks Road Cork City	Email – Dig@gasnetworks.ie	Received drawings outlining GNI services in the vicinity.
Irish Water P.O. Box 860, South City Delivery Office, Cork City	Email – planning@water.ie jagoold@water.ie	Extensive consultations which are ongoing

Table 2: Prescribed Bodies/External Consultations and Responses Received

Department of Arts, Heritage & Gaeltacht***Submission*****Proposed Development: Part 8 N21 Adare Western Approach Improvement Scheme**

A chara

On behalf of the Department of Arts, Heritage and the Gaeltacht, I refer to correspondence received in relation to the above. Outlined below are heritage-related observations/recommendations of the Department under the stated heading(s).

Nature Conservation

While not a legal requirement, it would be best practice to minimise or avoid as far as possible the removal or disturbance of hedgerows during the bird nesting season (March to August inclusive). You are requested to send further communications to Development Applications Unit (DAU) via eReferral, where used, or to manager.dau@ahg.gov.ie; if emailing is not possible, correspondence may alternatively be sent to:

The Manager
Development Applications Unit
Department of Arts, Heritage and the Gaeltacht
Newtown Road
Wexford
Y35 AP90

Designer's Response

It is intended to construct the main construction works for the project outside the tourist season, which broadly co-indices with the March-August window. The removal or disturbance of hedgerows, where practicable, will be undertaken outside the period requested.

Gas Networks Ireland***Submission Summary***

Gas Networks Ireland (GNI) submitted drawings pertaining to the existing gas network infrastructure within the scheme extents. The information is provided as a general guide and GNI noted in their submission that they cannot guarantee its accuracy and it should not be relied upon for accurate distance or depth of cover.

Designer's Response

The information provided in this submission will be included in the existing services location drawings which will be issued to all tenderers during the tender process.

RESPONSE TO SUBMISSIONS RECEIVED**Submission No 1. Keenan Property Management*****Submission Summary***

Ms. Catherine Dunford working on behalf of Keenan Property Management has requested Limerick City & County Council to investigate the possibility of constructing a yellow box at the signalised junction of the Village at Adare Manor and the N21. Ms. Dunford noted in her submission that when traffic backs up on the N21 it often blocks this junction and prevents traffic from exiting The Village or Manor Fields.

Designer's Response

The proposals have been amended to include for a yellow box at this junction.

Submission No 2. Fewer Harrington & Partners***Submission Summary***

Shane Lahiff, on behalf of Fewer Harrington & Partners, requested Limerick City & County Council to investigate the proposal put forward by Fewer Harrington & Partners regarding a joint access road off the N21 into the proposed car park which would also serve the proposed retirement village being promoted by FH&P.

Designer's Response

The matter of access to these lands must be subject to a separate planning application and discussions between Limerick City & County Councils Property Section.

Submission No 3. Adare Tidy Towns***Submission Summary***

Adare Tidy Towns, in their submission, requested that as much construction work as possible should be conducted at night to minimize the disruption to traffic flow.

Designer's Response

Limerick City & County Council will provide for certain construction activities relating to the main construction works to be carried out at night in order to minimize the disruption to traffic flow.

Submission No 4. Representation Adare Residents via Cllr. Adam Teskey***Submission Summary***

Adare residents via Cllr. Teskey have requested that Limerick City & County Council consider the proposal to construct a yellow box at the N21 / Blackabbey Road junction. At present motorists cannot join the N21 from the Blackabbey Road during periods of congestion.

Designer's Response

The proposals have been amended to include for a yellow box at this junction.

Submission No 5. Stephen Lavin – Scoil Naomh Íosaf***Submission Summary***

Mr. Lavin (Principal of Scoil Naomh Íosaf) welcomed the proposed works on the N21. He noted in his submission that the proposed car park across from the school and the widened footpaths are huge improvements to health and safety outside the school. Mr. Lavin requested that a vehicle actuated sign should be placed adjacent to the "SLOW" road marking outside the school.

Designer's Response

The proposals have been amended to include for a vehicle actuated speed sign.

Submission No 6. Karina Byrne – Scoil Naomh Íosaf Parent's Council***Submission Summary***

Ms. Byrne is Chairperson of Scoil Naomh Íosaf Parent's Council. In her submission she noted that the parent's council support the proposed changes to the N21. She also noted that health and safety would be improved with the addition of parking spaces.

Designer's Response

None

Submission No 7. Sean Collins, Sean Collins Pub, Adare***Submission Summary***

Mr. Collins requested a "Build out" in front of his premises to ensure sightlines are not impeded by parked vehicles when exiting the laneway adjacent to his property. Mr. Collins also requested that the car park be open all year.

Designer's Response

A build out in front of Sean Collins Bar will be considered as part of the detailed design. The car park will be available to the school and for business and public parking.

Submission No 8. Adare Community Council***Submission Summary***

In their submission, the Adare Community Council welcome the proposed works on the N21. The Adare Community Council requested diversion routes be signposted at Newcastle West, Limerick City, the M7, M18 and N20.

Adare Community Council also requested that the Foul Sewer be extended from the Village Hall to the Traffic Lights to accommodate all houses on the Rathkeale Road to Murphy's Cross, plus any future development on the southern/Adare Manor side of the traffic lights. The Foul Sewer also needs to be extended west beyond the lights to cater for future development on the council land at the Deerpark side. Ducting for all other services required for those developments should be included.

Designer's Response

Limerick City & County Council will signpost alternative routes during the construction stage for the scheme. Variable Message Signs will be provided at suitable locations in advance and during the course of construction for the main works contract. The purpose of these Variable Message Signs will be to provide advance notice to commuters from areas including Newcastlewest, Rathkeale, Limerick City / M20 / N69 to use an alternative route in order to avoid delays due to roadworks.

The provision of a Foul Sewer is a function of Irish Water. Limerick City & County Council have brought this submission to the attention of Irish Water. Extensive consultations have taken place between Irish Water and Limerick City & County Council with regard to the provision of a Foul Sewer.

The Council are of the view that any service, foul sewer or otherwise, planned for this section of the N21 should be installed as part of these road improvements and as such have, and will continue to give, favourable consideration to the inclusion of the foul sewer in the works.

Limerick City & County Council will consider the inclusion of a Foul Sewer along the N21 from Blackabbey Road as far as the Council lands located approximately 120m south-west of the Traffic Lights into the main contract for this scheme subject to the condition that all costs (including costs associated with the risks of constructing the Sewer) will be provided by Irish Water.

It is our understanding from discussions with Irish Water that a Foul Sewer from the Traffic Lights at the Village to Murphy's Cross could be provided in the future through the wayleave located behind the houses fronting the N21 between the Traffic Lights at the Village and Murphy's Cross.

Limerick City & County Council will provide ducting to accommodate future services.

Service Providers have been advised that services installed post construction will be subject to the provisions of Section 4.5.2 of the Guidelines for Managing Openings in Public Roads (Second Edition, September 2015). This requires that any services laid during a specified protected period will be subject to onerous reinstatement conditions (full width/lane/bay reinstatement etc.) as the Road Authority intends to ensure that such works do not compromise the structural integrity and design life of the new road pavements and footpaths.

Submission No 9 Tony Sexton, Ardshanbally, Adare, Co. Limerick***Submission Summary***

Mr Sexton welcomed the scheme in his submission. He made reference to the footpath extension and safety improvements to Scoil Naomh Íosaf. He noted the scheme should be futureproofed including provisions for foul sewer pipes, gas pipes and IT service ducting.

Designer's Response

The provision of a Foul Sewer is a function of Irish Water. Limerick City & County Council have brought this submission to the attention of Irish Water. Extensive consultations have taken place between Irish Water and Limerick City & County Council with regard to the provision of a Foul Sewer.

The Council are of the view that any service, foul sewer or otherwise, planned for this section of the N21 should be installed as part of these road improvements and as such have, and will continue to give, favourable consideration to the inclusion of the foul sewer in the works.

Limerick City & County Council will consider the inclusion of a Foul Sewer along the N21 from Blackabbey Road as far as the Council lands located approximately 120m south-west of the Traffic Lights into the main contract for this scheme subject to the condition that all costs (including costs associated with the risks of constructing the Sewer) will be provided by Irish Water.

Limerick City & County Council will provide ducting to accommodate future services.

Service Providers have been advised that services installed post construction will be subject to the provisions of Section 4.5.2 of the Guidelines for Managing Openings in Public Roads (Second Edition, September 2015). This requires that any services laid during a specified protected period will be subject to onerous reinstatement conditions (full width/lane/bay reinstatement etc.) as the Road Authority intends to ensure that such works do not compromise the structural integrity and design life of the new road pavements and footpaths.

8 CONCLUSION AND RECOMMENDATION

The construction of the Scheme will represent a significant improvement in the pavement quality for all road users on the N21 at this location. The Scheme will also represent significant improvements to safety particularly in the vicinity of Scoil Naomh Íosaf. It is recommended to proceed with the above development in accordance plan reference 15050-01-0019, Rev D and plan reference 15050-01-0020, Rev D contained in Appendix 3.

APPENDIX 1

Part 8 Particulars including:

Appendix A – Part 8 Notice & Drawings

**Appendix B - Appropriate Assessment Screening Report
& Cultural Heritage Assessment Report**



**N21 ADARE WESTERN APPROACH
IMPROVEMENT SCHEME**

**PLANNING AND DEVELOPMENT ACT, 2000-2015
PLANNING AND DEVELOPMENT REGULATIONS, 2001-2015**

**Report prepared pursuant to the requirements of Part 8,
Article 81 of the
Planning and Development Regulations, 2001-2015**

**Limerick City & County Council
7/8 Patrick Street
Limerick**

March 2016

N21 ADARE WESTERN APPROACH IMPROVEMENT SCHEME

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4	The Environment	4
5	Temporary Traffic Management.....	5

Appendix A:

Part 8 Notice

Part 8 A3 Drawings

Appendix B:

Appropriate Assessment Screening Report

Cultural Heritage Assessment Report

1 Background

The condition of the road pavement on the N21 National Primary road south of Adare town is poor and Limerick City & County Council, in conjunction with Transport Infrastructure Ireland, intends to implement a pavement improvement scheme.

Consent for the works is being sought under the Planning and Development Acts 2000-2015, and public consultation on the scheme is taking place pursuant to Part 8 of the Planning and Development Regulations 2001-2015.

2 Planning

As prescribed in the regulations, Limerick City & County Council has placed a site notice at the proposed scheme and advertised the proposed scheme in a local newspaper. A copy of the Part 8 Notice is included in Appendix A of this Report.

Plans and particulars of the proposed scheme will be available for inspection during normal office hours or purchase at a fee not exceeding the reasonable cost of making a copy at the offices of the Mid West National Road Design Office, Limerick City and County Council, Lissanalta House, Dooradoyle, Limerick at the Roads & Travel Department, Limerick City & County Council, County Hall, Dooradoyle, Limerick and at the Rathkeale Area Office, Limerick City and County Council, Áras Seán Finn, New Line Road, Rathkeale, Co. Limerick from Monday 14th March 2016 to Wednesday 27th April 2016.

Submissions or observations with respect to the proposed development dealing with the proper planning and sustainable development of the area may be made in writing to the Mid West National Road Design Office, Limerick City and County Council, Lissanalta House, Dooradoyle, Limerick on or before 4.30pm on Thursday 12th May 2016.

Submissions should be clearly marked:

N21 Adare Western Approach Improvement Scheme.

3 Nature and Extent of the Scheme

The scheme is located on the N21 within the Townlands of Adare, Graigue and Cummeen, on the southwest side of Adare village. It extends from the R519 Ballingarry Road (at southwestern end) to the L1422 Blackabbey Road (at the north-eastern end), over an approximate distance of 1.3 km.

The characteristics of the existing road are:

- it is a single carriageway without hard shoulders (but with hard strips along some sections);
- the speed limit is 60 km/h between the R519 Ballingarry Road and the school (Scoil Naomh Iosaf) and reduces to 50 km/h on the village side of the school;
- there is a signalised junction midway along the scheme providing access to The Village housing scheme;
- there is a footpath along most (but not all) of the south-eastern side of the road, and no footpath along most (but not all) of the north-western side.

Development has taken place along the roadsides, particularly along the south-eastern side. The development consists mainly of single detached houses but also includes a school and some business premises. On the north-western side, the Old Park estate bounds the road between the R519 and the traffic signals. To the northeast of the signals the lands are in agricultural use, with some housing and a nursing home.

The inadequacies of the existing road are:

- the road pavement is in poor condition; some sections have failed completely, particularly at the village end of the scheme; other sections require strengthening to carry the high level of traffic on the route;
- the road drainage is inadequate, and sections of the road are prone to flooding, particularly at the town end of the scheme;
- visibility for drivers exiting some side roads is restricted;
- the footpath network is not fully connected, and facilities for pedestrians are poor in some areas;

- the set-down area for the school (Scoil Naomh Iosaf) is located directly on the roadside of the N21. This layout results in congestion at school times and is a road safety concern.

A preliminary design has been developed and its principal characteristics are:

- resurfacing of the complete road (excluding the section of recently constructed pavement at the traffic signals);
- reconstruction of areas of road pavement to achieve acceptable design levels and to replace failed pavement;
- strengthening of other areas of pavement to extend design life;
- provision of a surface water drainage system with attenuated discharge and hydrocarbon interception;
- provision of a kerbed footpath along the southeast side of the road between the R519 and the traffic signal junction, and on the northwest side of the road between the signal junction and the School / Embury Close, to provide a fully connected footpath network;
- provision of a traffic calming island on the northeast-bound approach to the signalised junction, to reduce approach speeds;
- indenting of the drop-off area at the school to provide a buffer strip between the drop-off area and the N21;
- provision of an off-road public car park opposite the school (Scoil Naomh Iosaf);
- alteration of existing kerb lines and provision of new kerb lines to improve visibility from side roads and to provide a more uniform cross-section.

The nature and extent of the proposed scheme is shown on the following drawings which are included in Appendix A of this Report:

Drawing No.	Drawing Title
15050-01-0018	Scheme Location
15050-01-0019	Site Layout Plan <i>(Sheet 1 of 2)</i>
15050-01-0020	Site Layout Plan <i>(Sheet 2 of 2)</i>
15050-01-0023	Typical Cross-Sections

4 The Environment

Appropriate Assessment Screening:

Appropriate Assessment Screening of the proposed scheme was carried out by Limerick City & County Council in accordance with Article 6(3) of the EU Habitats Directive (92/43/EEC).

The assessment identified two sites located close to the proposed scheme:

- the Mague River – Lower River Shannon SAC site (No. 002165), located 500m from the scheme;
- the Adare Woodlands Proposed Natural Heritage Area, located beside the proposed scheme.

The assessment notes that:

- the scheme does not encroach onto either site;
- the proposed works are largely contained within the footprint of the existing road;
- surface water discharge will be attenuated and hydrocarbon interception will be used.

The assessment concludes that the impacts of the proposed works on ecology and hydrology are not considered to be significant. It concludes that full Appropriate Assessment is not required as the effects of the works on the Natura 2000 site are not judged to be significant.

Recommendations on mitigation measures emerged from the assessment. The measures relate generally to the construction stage of the scheme and will be incorporated into the design and specification of the works.

The Appropriate Assessment Screening Report is included in Appendix B of this Report.

Cultural Heritage Assessment:

A Cultural Heritage Assessment was also undertaken for the proposed scheme (Appendix B). It notes that the scheme is partly located within the zone of archaeological potential of the historic town and crosses the tentative line of the medieval town defences. It recommends that targeted test excavations be carried out in advance of construction, if feasible, at the point where the scheme crosses the possible line of the town wall. It also recommends that construction works within the zone of archaeological potential be archaeologically monitored under licence by a competent archaeologist.

The site of the proposed car park on the N21 Rathkeale Road, opposite Scoil Naomh Iosaf, is considered to be of archaeological potential and it is recommended that archaeological test excavations be undertaken in advance of construction to determine the presence or otherwise of any archaeological remains.

The proposed scheme does not impact directly on any buildings/structures of architectural heritage value, though there are many in close proximity to the scheme.

The Cultural Heritage Assessment Report is included in Appendix B of this Report.

5 Temporary Traffic Management

Traffic flow on the N21 through Adare is heavy: the current daily flow (the Annual Average Daily Traffic) is approximately 16,000 vehicles of which approximately 5% is heavy goods vehicles.

Most drivers travelling through Adare are on longer-distance trips. For that reason advisory diversions are intended as follows:

- westbound traffic diverted via N20 to Rourke's Cross and thereafter via R518 (for Rathkeale) or via R520 (for Newcastlewest and beyond);
- eastbound traffic diverted via R518 (Askeaton) and N69.

The local road network around the village is not capable of carrying diverted traffic from the N21 and for that reason is not being signed as a diversion route.

The following measures will be implemented, where feasible, to mitigate the impact on traffic flow during the main works:

- the main contract works would be undertaken during winter/spring (outside of the primary tourism season) when annual traffic volumes are lowest;
- lane closures on the N21 during the morning and evening rush-hours will be minimised;
- works that would require longer lengths of shuttle working (surfacing and delineation, for example), may be undertaken at night time.

APPENDIX A

Part 8 Notice And Part 8 A3 Drawings



LIMERICK CITY AND COUNTY COUNCIL

PLANNING AND DEVELOPMENT ACT 2000-2015

PLANNING AND DEVELOPMENT REGULATIONS 2001-2015

Part 8 Development

Development Description

In accordance with the Planning and Development Act 2000-2015, Article 81 of the Planning and Development Regulations 2001-2015, Limerick City and County Council hereby gives notice of the following proposed N21 Adare Western Approach Improvement Scheme.

The Works for the N21 Adare Western Approach Improvement Scheme will include resurfacing of the N21 from the L1422 Blackabbey Road to the R519 Ballingarry Road, reconstruction of sections of failed pavement, the provision of drainage systems with attenuated discharge and hydrocarbon interception, the provision of new and upgraded kerbing and footpaths, an off-road public car park opposite Scoil Naomh Iosaf and all ancillary works associated with the aforementioned in the townlands of Adare, Graigue and Cummeen, Co Limerick.

Plans and particulars of the proposed scheme will be available for inspection during normal office hours or purchase at a fee not exceeding the reasonable cost of making a copy at the offices of the Mid West National Road Design Office, Limerick City and County Council, Lissanalta House, Dooradoyle, Limerick at the Roads & Travel Department, Limerick City & County Council, Dooradoyle, Limerick and at the Rathkeale Area Office, Limerick City and County Council, Áras Seán Finn, New Line Road, Rathkeale, Co. Limerick from Monday 14th March 2016 to Wednesday 27th April 2016.

Submissions or observations with respect to the proposed development dealing with the proper planning and sustainable development of the area may be made in writing to the Mid West National Road Design Office, Limerick City and County Council, Lissanalta House, Dooradoyle, Limerick on or before 4.30pm on Thursday 12th May 2016. Submissions should be clearly marked: N21 Adare Western Approach Improvement Scheme.

LIMERICK CITY AND COUNTY COUNCIL

N21 Adare Western Approach Improvement Scheme

Part 8 Drawings



7 Ormonde Road
Kilmerly
Tel: +353 (0)57795500
e-mail: info@roadplan.ie



Limerick City and County Council
County Hall
Doonahyfe
Co. Limerick
tel: +353 81 496000
fax: +353 81 496001
e-mail: customer.services@limerick.ie



Mid West
National Road Design Office

Mid West National Road
Design Office
Lissanahin House
Doonahyfe Road
Doonahyfe, Limerick
tel: +353 81 496000
fax: +353 81 503150
e-mail: info@midwestroads.ie

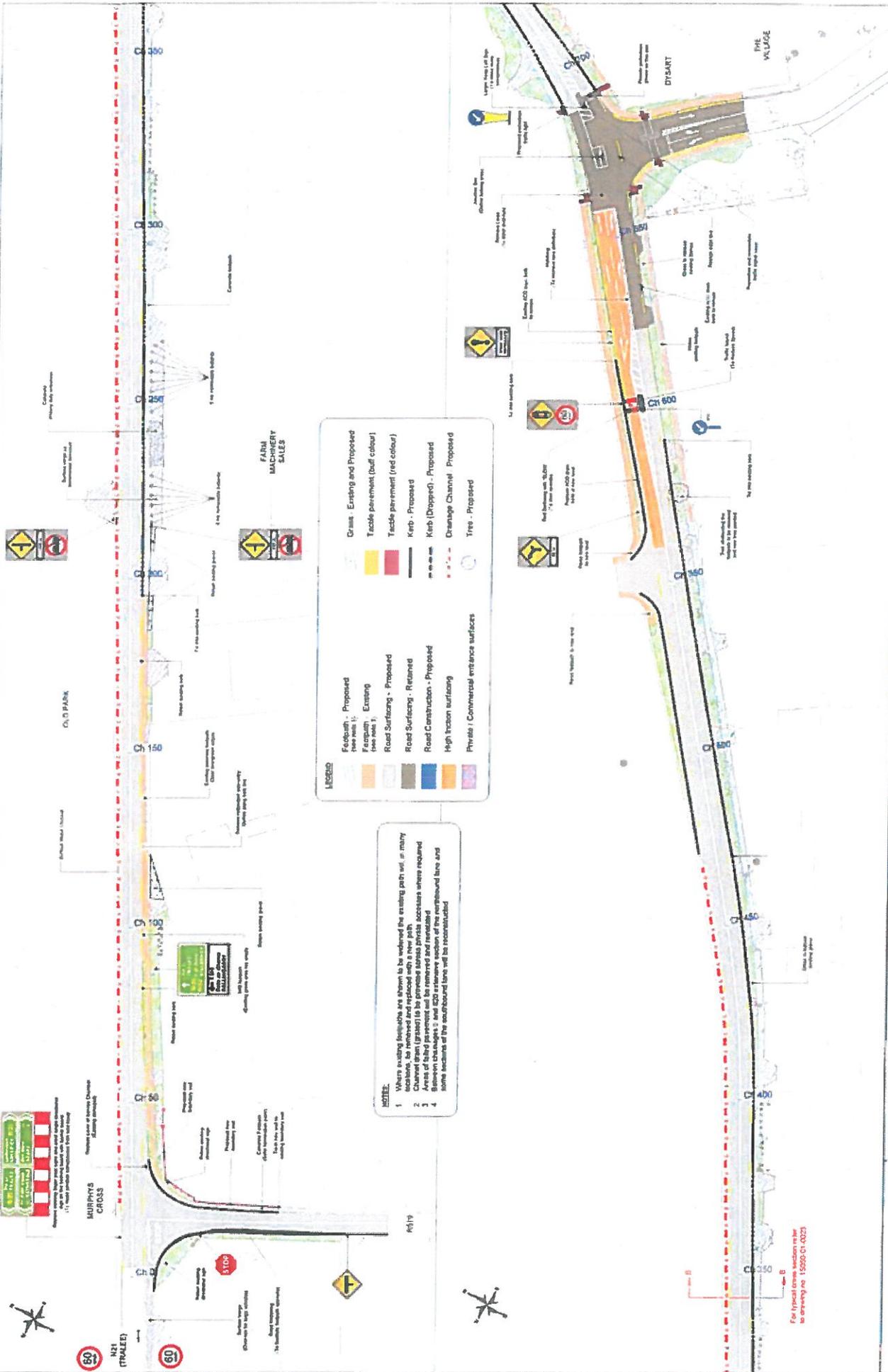


Transport Infrastructure Ireland
Parkgate Business Centre
Parkgate Street
Dublin 8
tel: +353 1 6602511
e-mail: info@tii.ie

N21 Adare Western Approach Improvement Scheme

Schedule of Scheme Drawings

- 15050 - 01 - 0018 - Scheme Location
- 15050 - 01 - 0019 - Site Layout Plan (Sheet 1 of 2)
- 15050 - 01 - 0020 - Site Layout Plan (Sheet 2 of 2)
- 15050 - 01 - 0023 - Typical Cross-Sections



LEGEND

Footpath - Proposed (see note 1)	Grass - Existing and Proposed
Footpath - Existing (see note 1)	Tactile pavement (buff colour)
Road Surfacing - Proposed	Tactile pavement (red colour)
Road Surfacing - Retained	Kerb - Proposed
Road Construction - Proposed	Kerb (Dropped) - Proposed
High friction surfacing	Drainage Channel - Proposed
Private / Commercial entrance surfaces	Tree - Proposed

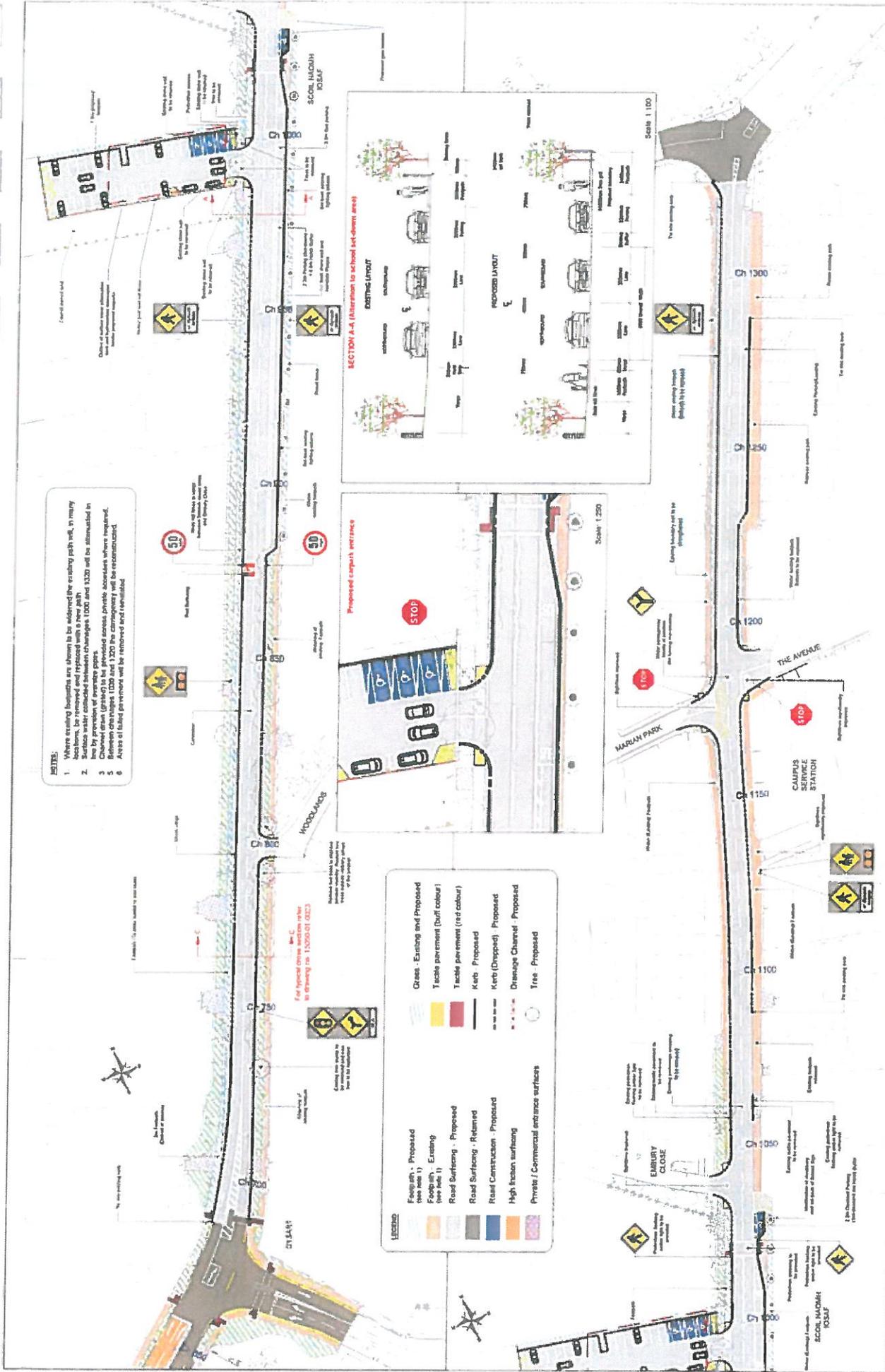
NOTES:

- 1 Where existing footpaths are shown to be widened the existing path will, in many locations, be removed and replaced with a new path.
- 2 Where existing footpaths are shown to be widened the existing path will, in many locations, be removed and replaced with a new path.
- 3 Where existing footpaths are shown to be widened the existing path will, in many locations, be removed and replaced with a new path.
- 4 Where existing footpaths are shown to be widened the existing path will, in many locations, be removed and replaced with a new path.

Some sections of the road/shoulder lane will be reinstated.

<p>7 Churchville Road Limerick Tel: 053 933 61 6000 Email: info@roadplan.ie</p>	<p>Limerick City and County Council County Hall Doreenstown Co. Limerick Tel: 053 933 61 6000 Email: customer.service@limerick.ie</p>	<p>Mid West National Road Design Office National Road Design Office Limerick Doreenstown Co. Limerick Tel: 053 933 61 6000 Email: info@midwestrdn.ie</p>	<p>Transport Infrastructure Ireland Pleasant Business Centre Doreenstown Dublin 6 Tel: +353 01 4802311 Email: info@tii.ie</p>	<p>PROJECT: N21 A690 Western Approach Improvement Scheme</p>
				<p>Drawing No: 016</p>

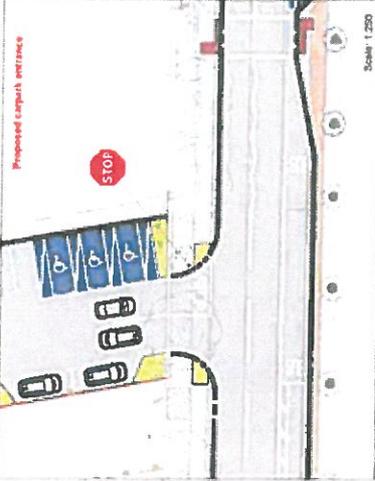
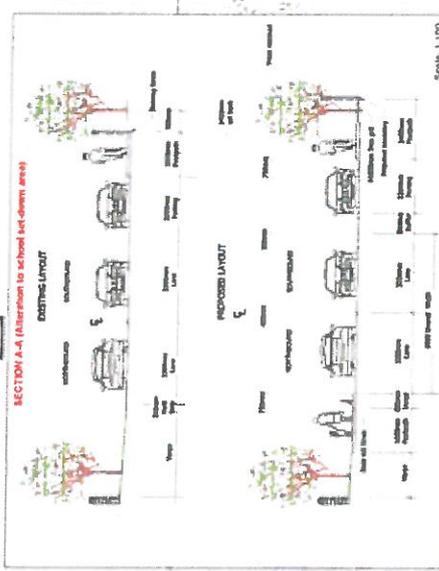
For typical cross sections refer to drawing no. 15050-C1-0023



- NOTES:**
- Where existing boundaries are shown to be adjacent the existing path will, in many locations, be removed and replaced with a new path.
 - Surface water collected between channels 1000 and 1320 will be attenuated in line with provisions of separate papers.
 - Proposed drainage channels are shown in blue. Areas of drainage private sewers where required, between channels 1000 and 1320 will be shown in red and will be reconstructed.
 - Areas of failed pavements will be removed and replaced.

LEGEND

Footpath - Proposed (see note 1)	Grass - Existing and Proposed
Footpath - Existing	Tackle pavement (buff colour)
Road Surfacing - Proposed	Tackle pavement (red colour)
Road Surfacing - Retained	Kerb Proposed
Road Construction - Proposed	Kerb (Disposed) - Proposed
High friction surfacing	Drainage Channel - Proposed
Private / Commercial entrance surfaces	Tree - Proposed



<p>ROADPLAN</p> <p>7 O'Connell Road, Galway Tel: +353 051 775500 e-mail: info@roadplan.ie</p>	<p>Limerick City & County Council</p> <p>Limerick City & County Council City Hall Co. Limerick Tel: +353 01 490001 e-mail: info@limerick.ie</p>	<p>National Road Design Office</p> <p>Mid West National Road Design Office Design Office Lincoln House Dunmore East Dromahaire, Limerick Co. Limerick Tel: +353 01 481850 e-mail: info@nrd.ie</p>	<p>TIA</p> <p>Transport Infrastructure Ireland Programme Business Centre Parkgate Street Dublin 8 Tel: +353 1 6002511 e-mail: info@tia.ie</p>	<p>Project Title: M21 Adams Western Approach Improvement Scheme</p> <p>Drawing Title: SHL Layout Plan (Sheet 3 of 3)</p> <p>Scale: A1</p> <p>Sheet: 1/208</p> <p>Date: February '16</p> <p>Author: [Name]</p> <p>Checker: [Name]</p> <p>Approver: [Name]</p> <p>Drawing Number: 10000-01-0000</p>
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APPENDIX B

Appropriate Assessment Screening Report

And

Cultural Heritage Assessment Report



Limerick City and County Council

Appropriate Assessment Screening.

N21 Adare Western Road Pavement Improvement Scheme.

February 2016.

Appropriate Assessment Screening – N21 Adare Western Road pavement Improvement Scheme.

The Appropriate Assessment Screening Document is for the improvement scheme on the western approaches to Adare which is on the N21 national primary route. The works will consist of re-surfacing a section of the existing road with the addition of signage, foot paths and traffic islands and new road markings as well as the construction of a drainage network, attenuation and car park. Construction of a drainage network and an attenuation system will also take place. It will take place along a 1.2km stretch of the N21, which lies within the settlement area of Adare. Some warning lighting will also be installed.

The nearest part of the work site is located 500m to the south west of the Maigue River which is part of the Lower River Shannon SAC site (002165).

Coordinates are as follows (Mid point of works): Discovery Series Map 65. X546166 Y645851. Please see drawing number 0107/56/0001 in the Appendix.

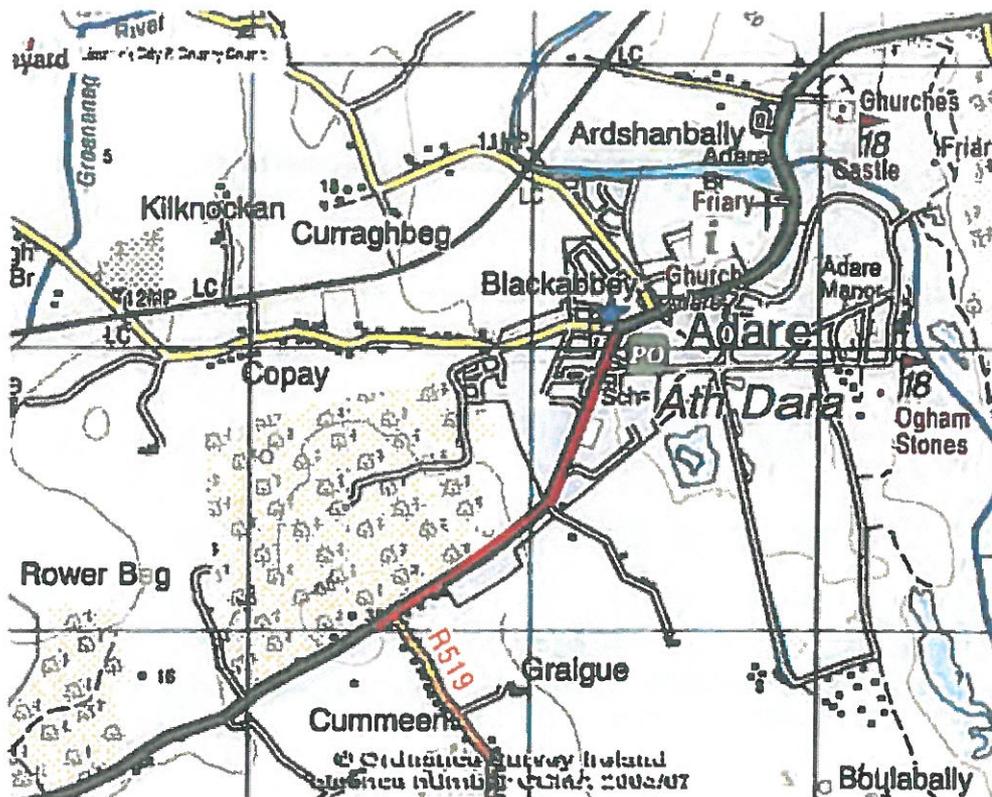


Figure 1: Showing Adare and the scheme location in red.

Assessment of the effects of the works:

1 While disturbance will be a factor during the construction works it will be of limited duration and will be 500m away from the river at it's closest point. Disturbance will also have to be viewed in the context of existing N21 Traffic which may help in masking the construction phase of the works. Much of the settlement of Adare lies between the area of works and the SAC site and will also help to mask any noise and activity resulting from the construction and operational phase of activities. The Adare woodlands Proposed Natural Heritage Area is just to the north of the scheme but the works will not encroach on it. It is separated from the works by a stone wall almost 2m in height which will help screen it from the works. The wall will also act as a barrier to mammal movement from the woodland which reduces risk to mammals such as Badgers (*Meles meles*).

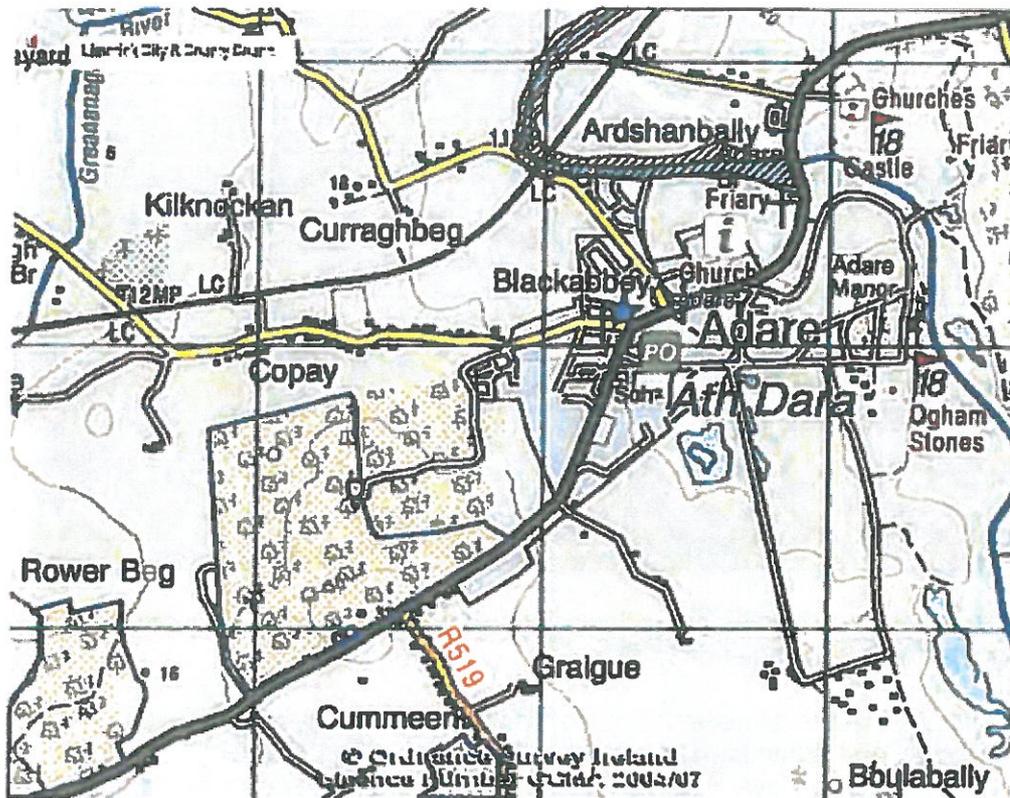


Figure 2: this shows the Adare Woodlands Proposed NHA (blue outlined) just to the north of the roadway with the Lower River Shannon SAC site running along the course of the Mulkear, to the north east.

2 Any of the habitats that are being modified for the works such as road side verges, built surfaces in the case of the existing road way and areas within the settlement are not of conservation concern.

3 No signs of badgers or their setts were found in the area of works. The area in the vicinity of the Adare Woodlands would have been the most likely area for them had they been present. While badgers are likely to use the woodlands to the north, the 2m high stone wall prevents their access to the roadway along the length of the works which parallel the woodland. This helps to reduce the potential for casualties. In one location the wall is reduced in height and is modern in construction allowing a recessed entrance way. This was inspected to see if there was any sign or tracks of badger usage. This was not the case.



Figure 3: showing the stone wall which borders the northern edge of the N21 along the Adare Woodlands NHA.

The woodland in this location is mix of species, some native such as Ash and Oak, Hazel and Whitethorn but with exotics such as Sycamore and Cedar also present. The shrub layer was very open with some ivy and Lords and ladies (*Arus maculatum*). The timing of the walk over (August) meant that earlier flowering plants would have been missed.



Figure 4: showing the open and generally young nature of the woodland portion closest to the road.

The woodland in this area is generally re-growth perhaps from earlier cutting. Much of the growth is multi-stemmed. Some felled trees remain on the ground and the area shows signs of wheel ruts where, presumably, the timber had been removed. This felling was not recent.

Conclusion: Full Appropriate assessment is not required as the effects of the works on the nearby Natura 2000 site are not judged to be significant.

Nearby Natura 2000 sites: The screening is in accordance with the requirements of Article 6(3) of the EU Habitats Directive (92/43/EEC). The principal consideration for an Appropriate Assessment would be if the proposed works were likely to have significant effects on a Natura 2000 site – Special Areas of Conservation and Special Protection Areas (SACs and SPAs) are Natura sites.

The nearest sites are the Lower River Shannon Special Area of Conservation Site (002165) and the Shannon and Fergus Estuaries SPA (004077) site, these are the closest in terms of distance. The road works parallel and are 500m from the Mague at their closest point which is part of the SAC site and is 7km distant from the SPA site to the north.

There are other sites within 36km of the proposed works area, these include the Slieve Felim SPA site (004165) an upland site with mixed forestry, upland grassland and peat habitats designated for the hen harrier. Curraghchase Woodlands site is to the west and has been designated for the Lesser Horseshoe

Bat (Site Code 0000174) and is a mixed woodland site. Also to the west is the Askeaton fen complex (002279) a calcareous fen complex. The small scale of the works involved in the current proposal means that there will not be an effect on these sites. They also involve modifications to a previously constructed roadway so in the larger scheme of things the effects of the works are not likely to be significant.

Screening Matrix

<p>Brief description of the project:</p>
<p>The works will consist of re-surfacing a section of the existing road with the addition of signage, foot paths and traffic islands and new road markings and a drainage network and attenuation system and car park. It will take place along an 1.2km stretch of the N21. All the works lie within the development boundary of the Adare Local Area Plan. Warning lighting for pedestrians will also be installed.</p>
<p>Brief description of the Natura 2000 sites:</p>
<p>The works are to the south west of the Mague River which is part of the Lower River Shannon SAC site (002165) designated for a range of riparian habitats. It is valuable for salmonid species and lamprey and for mammals such as the otter (<i>Lutra lutra</i>). At this stage in the course of the river it is subject to tidal influences.</p> <p>There are other sites within 36km of the proposed works site, these include the Slieve Felim SPA site (004165) an upland site with mixed forestry, upland grassland and peat habitats designated for the hen harrier. Curraghchase Woodlands site is to the west and has been designated for the Lesser Horseshoe Bat (Site Code 0000174) and is a mixed woodland site. Also to the west is the Askeaton fen complex (002279) a calcareous fen complex.</p> <p>Slieve Felim to Silvermines Special Protection Area (004165) lies 36km to the east. This is designated for the conservation of the hen harrier. Approximately half the area is afforested, with commercial plantations. Remaining open upland areas within the SPA assume greater importance as a result of this with upland grasslands, cutaway and intact bogs providing habitats for foraging activities in particular.</p> <p>To the immediate north of the roadway is part of the Adare Woodlands proposed Natural History Area (00429). Though parts of the different components of the site are believed to be ancient woodland, this portion looks as if it had been cut in the recent past and has subsequently regenerated.</p>

Describe the individual elements of the project (either alone or in combination with other plans or projects) likely to give rise to impacts on the Natura 2000 site:

The main way in which the works could have an effect on the SAC site would be through contamination which might reach the river channel through water courses which lead from the road way to the river channel. Drawings 15050-01-0019 and 15050-01-0020 in the Appendix shows the proposed drainage lay out. The eastern portion of the works, from the school to the town hall, will drain to a storm sewer which will discharge to a water course in the centre of the village which in turn leads to the Maigue. There is no space in which to provide tanks for attenuation or interception along the eastern portion of the works so the design proposed is threefold: provide oversize pipes for attenuation, provide a much increased number of gullies for sedimentation (within sumps) and ensure that all gullies within this are trapped so that they intercept minor spillages.

Both of the above measures would ensure that the risks of pollution and the risk of altering the flow regime of the Maigue are reduced to a level that would not be deemed significant.

For the western portion of the scheme, discharge will take place to an attenuation area which will be located opposite the National School. Similar to the eastern portion of the scheme run off from the roadway will be diverted through sediment and oil traps prior to discharge to the attenuation area. The discharge will be managed to ensure that it does not exceed green field rates.

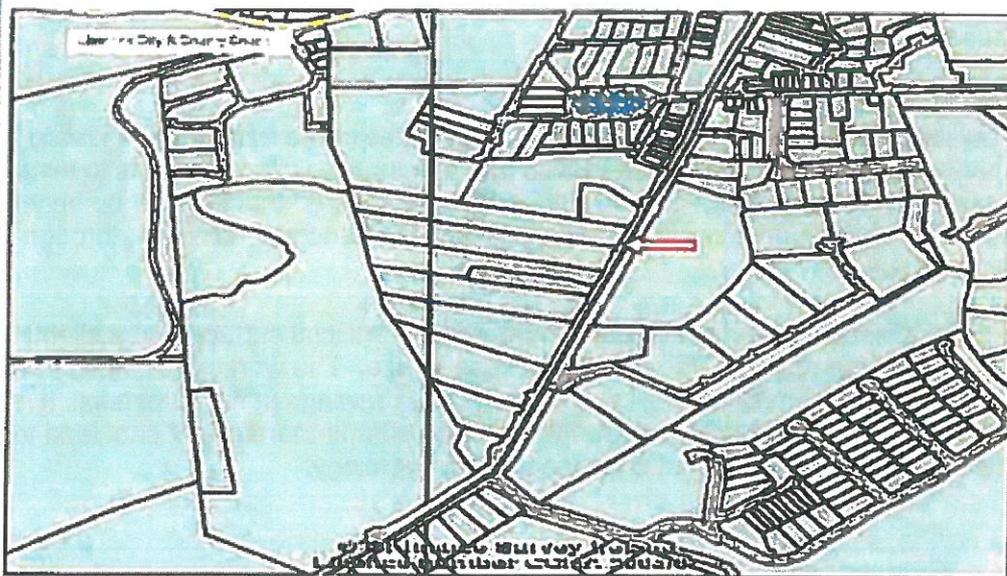


Figure 5: showing location of the attenuation area (arrowed) proposed to deal with run off from the western portion of the scheme. This is located outside any designated site and the plot is currently grassland.

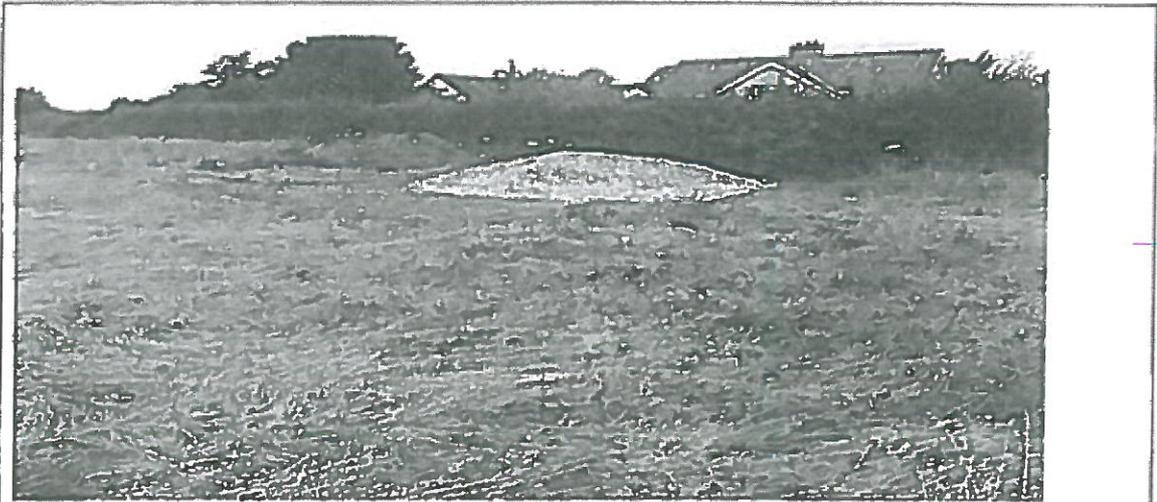


Figure 6: showing the test area for the attenuation area and car park, photographed on August 14th 2015.

The attenuation area and car park is located on an area of improved grassland which has not been cut this year for hay or silage which is the common practice for the plots to the west of this one.

A quick examination of the grass species indicated that common species such as Cocksfoot (*Dactylis glomerata*) Annual Meadow Grass (*Poa annua*) and Yorkshire fog (*Holcus lanatus*) were present. These are common species in road side verges or underutilised grasslands.

Additional notes:

As outlined earlier the works have to be viewed in the context of the pre-existing N21 Route way and it's traffic. However given that the purpose of the works is to install traffic calming measures and reduce traffic speed it is considered that it would be beneficial for wildlife such as badgers or otters by minimising the chances of collision, through reduced speeds.

The works also involve modifications to an existing artificial surface and a slight expansion to it a roughly five percent increase in size. The works are limited in scale and confined to a specific area - see Figure 1 and drawings in the Appendix. It is not considered that this will appreciably increase run off and therefore will not lead to localised flood risk or material changes to local hydrology.

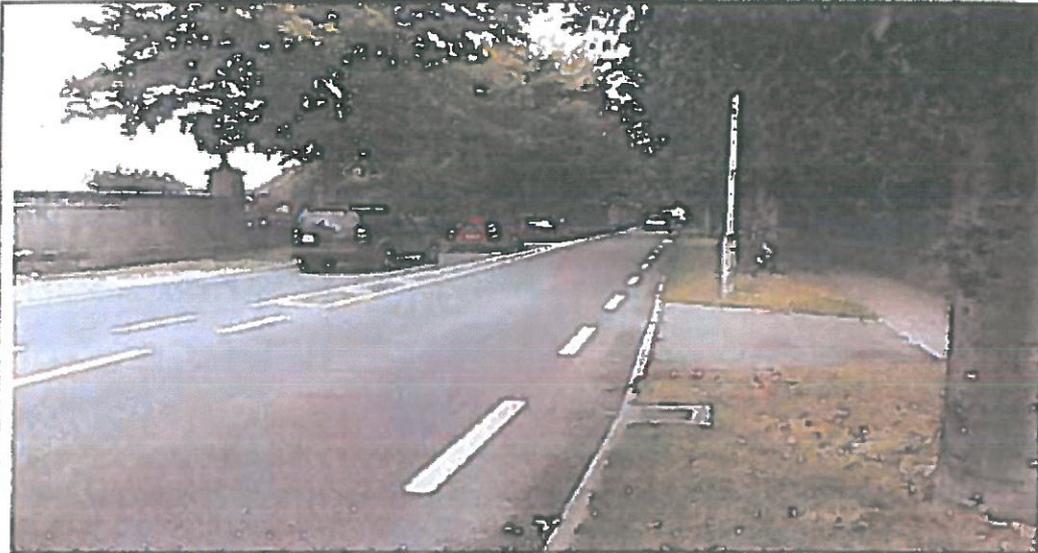


Figure 7: showing the N21 looking eastwards. This indicates that the works proposed will be alterations to an existing national roadway.

Considering the above, including the fact that works are modifications to an existing route-way and do not encroach into designated sites, it is considered that the ecological effects of the works are not significant. In terms of ex-situ effects the release of contaminants will be controlled by the presence of silt and oil traps and the attenuation proposed will reduce run off to that of green field rates which should mean that localised effects on local hydrology are minimised. The dangers of flood risk as examined by the Flood Risk Assessment document produced for the scheme (Roadplan July 2015, 15050-01-005) is not considered significant. The route is already lit and the alteration in lighting is not considered to add significantly to the disturbance effects for bats. It is recommended that where possible and consistent with road safety that bat friendly lighting is used.

Mitigation measures such as the following will minimise the chances of any pollution entering the water course.

- 1 All fuels, oils and lubricants to be stored at least 20m away from any water course. No refuelling of vehicles or servicing to take place within 20m of any water course.**
- 2 No tarring or works to take place outside the areas indicated as being subject to works on the site drawings.**
- 3 Tarring and works in the vicinity of any water course (i.e. within 20m) to cease during heavy rains.**
- 4 No dumping of any materials in water courses or outside the area of works. All waste**

to be disposed of in an approved facility and no empty containers or residue to remain on site following completion of works.

5 . It is recommended that where possible and consistent with road safety that bat friendly lighting is used.

Describe any likely direct, indirect or secondary impacts of the project (either alone or in combination with other plans or projects) on the Natura 2000 site by virtue of:

- **Size and scale:**

Works are outside the SAC site and are 1.2 km length and will cover an area of an average width of 10.2m. The distance from the SAC site means that the localised effects of these works on an existing roadway will not have significant ecological effects as the habitats in these areas are highly modified and heavily trafficked and as such have limited ecological value. There will be no incursion into the Adare woodlands NHA site on the western portion of the scheme which is protected from the works on the existing road by a stone wall of 2m height.

- **Land-take;**

None within any designated site either PNHA or SAC. Improved grassland will be removed to make way for the attenuation area and car park. This is a non designated area and as such there will be no reduction in land area within any Natura 2000 site.

- **Distance from Natura 2000 site or key features of the site:**

Between 400 and 800m. Much of the intervening area is built up and forms an effective screen from the works area. .

- **(water abstraction etc);**

None.

- **Emission (disposal to land, water or air);**

There is the risk of emissions from the roadway though this has been taken into consideration by the provision of silt and oil traps for run off from the roadway. The run off will be attenuated so the rate will not exceed green field site run off rates which should further minimise effects on local hydrology.

- **Excavation requirements;**

None within the SAC site, some excavation along the works area. This is not likely to cause any effects on the site due to the limited scale of the excavations. The habitats which are affected are roadside verges or existing road surfaces which are of limited ecological value. There will possibly be some rock breaking in the vicinity of the attenuation area and in the carriage way for the installation of the storm sewer network. In the longer term this is expected to have beneficial effects on the SAC site as this will reduce flow rates from the impermeable surface area and with the sediment and oil traps instated will ensure that potential pollution leakage to ground water and indirectly to the SAC site will be minimised. This should result in an improvement in existing situation where such updated mitigation measures are not in place.

Transportation requirements;

Materials and machinery, grit, bitumen, and signage will be brought to the site along existing public roads. This will not have an effect on the SAC site.

- **Duration of construction, operation, decommissioning, etc;**

Three to four construction month period for construction. Operational phase is expected to be indefinite as the N21 is a nationally important route way

Other

None.

Additional notes: JBA flooding map is included below- the light blue zone indicates Flood Zone A and the darker blue area is Flood Zone B. Flood Zone A defines areas with the highest probability of flooding from rivers (i.e. more than 1% probability or more than 1 in 100). Flood Zone B defines areas with a low probability of flooding from rivers (i.e. less than 0.1% probability or less than 1 in 1000). However this is not supported by evidence from the 6 inch map series which does not show this area as being prone to flooding. Consideration of the on site flood regime is important in that flooding can cause the spreading of contaminants.

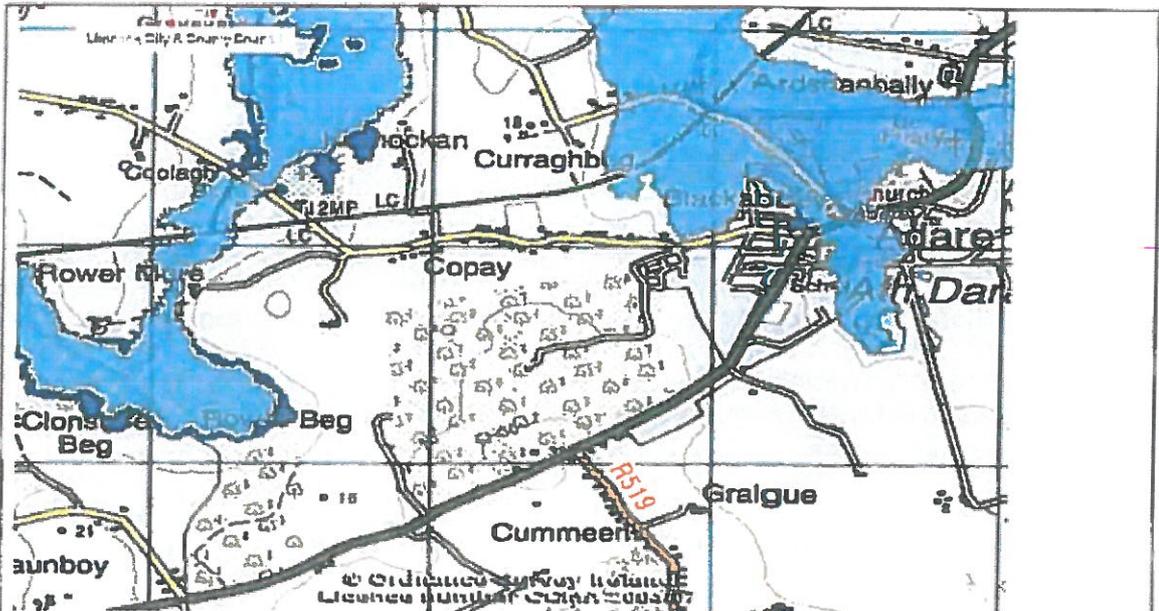


Figure 6: Indicating predictive flood extents.

The predictive flood maps do not show part of the area of works as being subject to flooding and there has been no record of flooding of the road way. The limited expansion of the already largely impermeable area of the roadway i.e. approximately 5% is not expected to cause any significant run off which would alter flow patterns in the Mauge river.

Describe any likely impacts on the Natura 2000 site as a whole in terms of:

- interference with the key relationships that define the structure of the site;

None- any effects are limited both by the distance from the SAC site, the intervening buffer of the settlement of Adare, while the limited scale of works limits the potential ex-situ effects.

- interference with key relationships that define the function of the site;

None- see above

Provide indicators of significance as a result of the identification of effects set out above in terms of:

- loss;

The works are outside the SAC site and will not result in habitat loss within the site itself. As indicated elsewhere the habitats being affected are common and are not of

ecological significance when viewed in the context of the conservation interests of the lower River Shannon SAC site.

The Adare woodlands site, though not part of the Natura 2000 network, will not be encroached upon as the works will take place within the existing development footprint of the road and its verges.

- **Fragmentation;**

The issue of the roadway acting as a barrier to badger and a lesser extent otter movements outside the SAC site has been raised above. The effects of slowing traffic within the area of works is expected to reduce the potential for otter and badger mortality in the area.

- **Disruption;**

No effects anticipated- the works are at a distance from the SAC site- 500m at its closest and are visually screened from the site by the intervening parts of the Adare settlement.

- **Disturbance;**

None- the site is at a distance from the SAC site and is an already busy national primary route. The effects of the construction phase are not expected to add appreciably to any disturbance effects. Traffic calming following completion, would it is anticipated, slow traffic slightly which might result in marginally less traffic noise.

- **change to key elements of the site (e.g. water quality etc);**

None, effects of the works are expected to be temporary. See above for mitigation measures re works. In addition precautions are to be taken to prevent the release of contaminants to local water courses- see above.

Describe from the above those elements of the project or plan, or combination of elements, where the above impacts are likely to be significant or where the scale or magnitude of impacts are not known.

The works themselves are of limited area and are not likely to have any effects on the SAC or SPA sites as they are at a distance from the sites-500m at its closest point from the SAC site and 7km from the SPA.

Finding of No Significant Effects Matrix

<p>Name of Project:</p>	<p>N21 Adare Western Road Pavement Improvement Scheme.</p> <p>The works will consist of re-surfacing a section of the existing road with the addition of signage, foot paths and traffic islands and new road markings, drainage, car-park and attenuation area. It will take place along a 1.2 km stretch of the N21, some of which lies within the settlement area of Adare. Additional warning lighting will be installed. A drainage network and attenuation system will also be constructed.</p>
<p>Name and location of Natura 2000 sites:</p>	<p>Within 500m of the Lower River Shannon SAC site, while it is 7 km south east of the Shannon and Fergus Estuary SPA.</p>
<p>Description of the Project or Plan</p>	<p>See above</p>
<p>Is the Project or Plan directly connected with or necessary to the management of the site (provide details) ?</p>	<p>No</p>
<p>Are there other projects or plans that together with the project of plan being assessed could affect the site (provide details)?</p>	<p>No</p>

The Assessment of Significance of Effects	
Describe how the project or plan (alone or in combination) is likely to affect the Natura 2000 sites:	<p>It may affect the site through pollution through run off into water courses that discharge to the SAC site. The roadway may also acts as a barrier to otter movement to and from the Maigue</p>
Explain why these effects are not considered significant:	<p>The slowing of traffic as result of the traffic calming works is expected to have beneficial effects on otter and badger mortality on this stretch of roadway. Otter are not the most road wise of small mammals.</p> <p>The inclusion of oil and sediment traps within the works are indeed to reduce the amount of runoff and contaminants that might end up in local water courses.</p> <p>The works themselves involve relatively minor modifications to an existing route way which is already highly modified. The area of the works lies within the settlement area of Adare village which is already subject to disturbance and is highly modified. These factors allied to the distance from the SAC site indicates that the effects of the works are not significant.</p> <p>Refuelling and servicing and storage of fuel, oils or chemical are to take place a minimum of 20m from the waters edge. This is to minimise the chances of water pollution.</p> <p>Road resurfacing to take place in dry conditions with care being taken in application of surface dressing to minimise the risk of water pollution.</p>

	It is recommended that where possible and consistent with road safety that bat friendly lighting is used.		
List of Agencies Consulted: Provide contact name and telephone or email address:	<i>The Manager, Development Applications Unit DoEHLG Newtown Road, Wexford. (T: 053 9117382)</i>		
Response to consultation	<i>Awaited.</i>		
Data Collected to Carry out the Assessment			
Who carried out the Assessment?	Sources of Data	Level of assessment Completed	Where can the full results of the assessment be accessed and viewed
Heritage Officer, Forward Planning Section, Limerick County Council.	Existing NPWS Site Synopses Site visits and site surveys. Road Design office,	Desktop study, site visits	The conclusions are included in the screening document and the drawings are attached in an Appendix to this document.

N21 Adare Western Approach Improvement Scheme
County Limerick
Cultural Heritage Assessment



Mid West
National Road Design Office



Date of Report: 10 March 2016

Report Author: Paul O'Keeffe

Report Status: Final

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*N21 Adare Western Approach Improvement Scheme
Cultural Heritage Assessment*

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1. Introduction

The aim of the proposed N21 Adare Western Approach Improvement Scheme is to upgrade the road pavement, footpaths and drainage of the present N21 Rathkeale Road within Adare Town, Co. Limerick. The scheme extends for approximately 1.4 km through the townlands of (from north-east to south-west) Adare, Graigue and Cummeen. The centre point of the scheme is at 546028 E, 645526 N (ITM); it is located on sheet 21 of the Ordnance Survey six-inch to one mile scale survey of Co. Limerick. This report describes the results of a desk-based study of the potential cultural heritage impacts of the scheme and makes recommendations as to how these impacts may be avoided or reduced.

2. Description of Proposed Works

The proposed scheme extends south-west from the junction of the Rathkeale Road and the L1422 Blackabbey Road to the junction of the Rathkeale Road with the R519 Ballingarry Road. It is c. 1.4 km long and is confined to the existing road footprint, including footpaths. The proposed works will comprise, but are not limited to:

- A combination of pavement overlay/inlay and possibly some road reconstruction dependant on pavement testing results.
- Replacement of some of the existing footpaths and the construction of new footpaths within the existing verges.
- The construction of a new storm carrier pipe network (preferably within the verges, subject to confirmation of utility locations).
- Provision of an off-road car park on the western side of the Rathkeale Road, opposite Scoil Naomh Iosaf

3. Legislative Background

All archaeological monuments listed on the Register of Historic Monuments (RHM), the Record of Monuments and Places (RMP) or the register of national monuments are protected under the National Monuments Acts 1930–2004. A monument—as defined by Section 2 of the Act—includes, in effect, all man-made structures of whatever form or date except buildings habitually used for ecclesiastical purposes. Further definitions of terms such as archaeology, archaeological object, architectural heritage, heritage building, heritage gardens and parks, heritage objects and monuments are provided by the Heritage Act 1995.

Section 12(1) of the National Monuments (Amendment) Act 1994 provided for the establishment of the RMP. Monuments listed in the RMP and marked on the RMP maps are known as recorded monuments. Under Section 12, any works in relation to such a monument require two months notice to the Minister of the Department of Arts, Heritage and the Gaeltacht (DAHG).

Under Section 14 of the National Monuments (Amendment) Act 2004, works in the vicinity of a national monument of which the Minister or a local authority are the owners or the guardians or in respect of which a preservation order is in force, require Ministerial Consent.

The Planning and Development Act 2000-2010 requires that a local authority set up and maintain a Record of Protected Structures which consists of structures that are of special architectural, historical, archaeological, artistic, cultural, scientific, social or technical interest. Designation confers protection on land and structures within the curtilage of the Protected Structure. Curtilage is not defined within the legislation but can be taken to include the parcel of land immediately associated with the structure and which is (or was) in use for the purposes of the structure (DoEHLG 2004, 191). In the case of a country house the curtilage may include such features as the stable buildings, coach-house, gate piers, gates, walled garden and lawns, unless they are located at a distance from the main building.

Attendant grounds are defined as lands which lie outside the curtilage of a protected structure, but are intrinsic to its appreciation, function or setting (*ibid.*, 192). The entire demesne of a country house may be considered as attendant grounds, along with any structures/features within it such as follies, plantations, earthworks and lakes. However, structures/features within the attendant grounds are not automatically protected and require specific inclusion in the Record of Protected Structures (*ibid.*).

The Planning and Development Act 2000-2010 also requires that a planning authority include an objective in its development plan to preserve the character of a place, area, group of structures or townscape if it is of the opinion that its inclusion is necessary for the preservation of the character of that area. Such an area is known as an Architectural Conservation Area (ACA) and is defined as a place, area, group of structures or townscape that is of special architectural, historical, archaeological, artistic, cultural, scientific, social or technical interest or that contributes to the appreciation of a protected structure (*ibid.*; Cork County Council [undated]).

4. Planning Policy

The Limerick County Development Plan 2010–2016 sets out the Council's objectives in terms of the protection and promotion of cultural heritage. The objectives of particular note with regard to the present scheme are set out below (after Limerick County Council 2010, vol. 1, 7-23–7-26).

4.1. Archaeological heritage

Objective EH O25: Preservation of the Archaeological Heritage

It is the objective of the Council to seek the preservation (*in situ*, or at a minimum, preservation by record) of all known sites and features of historical and archaeological interest. This is to include all the sites listed in the Record of Monuments and Places as established under Section 12 of the National Monuments (Amendment) Act 1994.

Objective EH O26: Preservation of the unrecorded/newly discovered archaeological heritage

It is the objective of the Council to protect and preserve (in situ, or at a minimum, preservation by record) all sites and features of historical interest discovered subsequent to the publication of the Record of Monuments and Places.

Objective EH O27: Protection of the setting of archaeological monuments

It is the objective of the Council to ensure that any proposed development shall not have a negative impact on the character or setting of an archaeological monument.

4.2. Architectural heritage

Objective EH O31: General Protection of Structures

It is the objective of the Council to:

a) seek the protection of all structures (or, where appropriate, parts of structures) within the County, which are of special architectural, historical, archaeological, artistic, cultural, scientific, social or technical interest and listed in the Record of Protected Structures. The record will continue to be developed on an ongoing basis, as resources permit, in accordance with the criteria laid down in the Architectural Heritage Protection Guidelines for Planning Authorities.

b) As resources permit, determine the extent of the curtilage of protected structures.

Objective EH O33: Re-use of Protected Structures

It is the objective of the Council to encourage the re-use of protected structures while recognising that such an objective will require adaptation and modifications of the building, including the construction of extensions. However, the Planning Authority will require the maintenance of essential architectural character, retention of features of special interest and respect for the structure's fabric, plan, form and setting. The development of, or on, a protected structure which would have a significant adverse impact upon its character will not be permitted. Developments in proximity to a protected structure, which would seriously affect its character, amenity, or setting will not normally be permitted.

Objective EH O34: Establishment of Architectural Conservation Areas

a) It is the objective of the Council to identify places of special architectural, historical, archaeological, artistic, cultural, scientific, social or technical interest and to define them as Architectural Conservation Areas. Additional Architectural Conservation Areas may be identified and included during the lifetime of the Plan.

b) It is an objective of the Council, during the life time of the Development Plan, to undertake a detailed survey and appraisal of all Architectural Conservation Areas designated under the Development Plan, where appropriate. A set of both general and site specific policies, objectives and guidelines will then be formulated for each Architectural Conservation Area. These measures will be adopted as a means of ensuring the continued preservation, protection and enhancement of the special character of our Architectural Conservation Areas.

Objective EH O35: Protection of Architectural Conservation Areas

a) The Planning Authority will seek to protect buildings of merit that demonstrate past design or construction practices within an Architectural Conservation Area as this can damage the integrity of an area.

b) Conservation, restoration and reconstruction is the preferred approach in Architectural Conservation Areas.

c) The Planning Authority will not permit insensitive developments that compromise the character or integrity of Architectural Conservation Areas and will seek to have large-scale developments respect the morphology and layout of a town or village.

d) Development proposals on sites in the vicinity of an Architectural Conservation Area will only be permitted where it can clearly be demonstrated that the development will not materially affect the special character or the integrity of the area, its amenity and setting. The special character of an area includes its traditional building stock and material finishes, spaces, streetscape, landscape and setting.

Objective EH O37: General Protection of Architectural Streetscapes

It is the objective of the Council to protect the character of the medieval towns, post-medieval features and 18th–19th streetscapes in the County having due regard to their architectural character, plot pattern and street patterns when assessing planning applications for development.

4.3. Policy and objectives of the Adare Local Area Plan 2015–2021

The Adare Local Area Plan 2015–2021 sets out the Council's specific objectives in terms of the protection and promotion of the cultural heritage of Adare. The objectives of particular note with regard to the present scheme are set out below (after Limerick City and County Council 2015, 50–61).

Policy EH 1

It is the policy of the Council to ensure that the archaeological, architectural, natural and built heritage of Adare is protected.

Policy EH 2

It is the policy of the Council to ensure that all proposals shall comply with the policies, objectives and development management standards of the Limerick County Development Plan 2010-2016.

Objective EH 2: Architectural Conservation Area (ACA)

It is the objective of the Council to protect, conserve and where appropriate, enhance the ACA as identified in Map 4. Proposals for development within the ACA shall

a) Reflect and respect the scale and form of existing structures within the ACA in proportioning, overall scale and use of materials and finishes, particularly with reference to the street frontages and seek to contribute to or enhance the character and streetscape of the ACA;

b) Seek to retain/incorporate/replicate exterior features which contribute or enhance the character and streetscape of the ACA such as shop fronts, sash windows, gutters and down pipes, decorative plasterwork etc;

c) Ensure priority is given to the pedestrian, to inclusive access, and to facilitating the improvement of the quality of the public realm.

Objective EH 3: Protected Structures

It is the objective of the Council to protect structures entered onto the Record of protected structures, or listed to be entered onto the Record and to encourage their appropriate re-use and restoration. The Council shall resist;

a) Demolition of protected structures, in whole or in part;

b) Removal or modification of features of architectural importance;

c) Development that would adversely affect the setting of the protected structure.

5. Methodology

A desktop survey of known archaeological sites and sites of archaeological and cultural heritage potential within the study area was carried out in order to assess the potential impact of the proposed scheme on the cultural heritage resource. The following sources were consulted:

- *Sites and Monuments Record (SMR) and the Record of Monuments and Places (RMP) for County Limerick (<http://webgis.archaeology.ie/NationalMonuments/FlexViewer/>)*
- published *Record of Monuments and Places* maps for County Limerick
- *National Monuments in State Care: Ownership and Guardianship*
- *National Monuments subject to Preservation Orders*
- National Inventory of Architectural Heritage (www.buildingsofireland.ie)
- Placenames Database of Ireland (www.logainm.ie)
- online excavations bulletin (www.excavations.ie)
- Limerick County Development Plan 2010–16
- Adare Local Area Plan 2015–2021
- 1st edition Ordnance Survey six-inch mapping (1844), 1st edition Ordnance Survey 25-inch mapping (1901) and orthophotographs (1995, 2000 and 2005) (<http://www.maps.osi.ie/publicviewer>)

6. Cultural Heritage Assessment

6.1. Archaeological and historical background

The proposed scheme is situated in the townlands of Adare, Graigue and Cummeen, the civil parish of Adare and the barony of Coshma. Adare derives from the Irish *Áth Dara*, meaning the 'ford of the oak' (www.logainm.ie/en/30472), presumably referring to the fording point on the Maigue which influenced the location of the town. Graigue derives from *An Ghráig*, meaning 'the hamlet/cattle-steading' (www.logainm.ie/en/3559) and Cummeen translates as *An Coimín*, meaning 'the commonage' (<http://www.logainm.ie/en/30480>).

Adare Town originated as an Anglo-Norman settlement, established at a strategic crossing point of the River Maigue. It was one of the many urban centres established by the Anglo-Normans in County Limerick, which also included the towns of Kilmallock, Askeaton, and Rathkeale, and the smaller boroughs of Ardagh, Castletown, Caherconlish, Croagh, Croom, Effin, Castletown Conyers, Clonshire, Galbally, Glenogra, Knockainy, Newcastle West, Mungret, Pallas Grean and Shanagolden (Bradley *et al.* 1989). These settlements quickly became the focus of economic, political and ecclesiastical activity in the region and served to consolidate and centralise the power of the Anglo-Norman lords.

Adare was a principal manor and appears to have obtained borough status by the early-13th century, as indicated by the permission granted to Geoffrey de Marisco to hold a fair there in 1226 (Keegan 2005, 25). By this time, the settlement already boasted a castle (probably built on an earlier ringwork), a parish church and a Trinitarian Abbey (known as the 'White Abbey'), the latter being the only foundation of that order established in Ireland (Bradley *et al.* 1989; Halpin and Newman 2006, 370). Adare obtained a grant of murage to fund the construction of town walls in 1310 and there is a reference to the repair of these walls in 1376 (Bradley *et al.* 1989, 3).

The castle, known as 'Desmond Castle', is situated on low-lying ground on the north bank of the Maigue. It consists of a three-storey, square keep set within a sub-rectangular enclosure defined by an inner ward and an outer ward (*ibid.*, 3). It was described in 1331 as comprising of a "C. in which there is a hall; a C. with stone walls, covered with thatch; a tower, covered with planks; a kitchen, covered with slates, and a chamber near the stone part covered with thatch" (Westropp 1906/1907, 162). It was restored by the Office of Public Works in the 1990s and is designated as a national monument (National Monument no. 622).

From c. 1230, the manor of Adare was owned by the Fitzgerald earls of Kildare (Thomas 1992, cited in Limerick City and County Council 2015, 10) and it was John FitzThomas FitzGerald, the first earl, who founded the Augustinian Friary on the south bank of the Maigue, some time before 1316 (Gwynn and Hadcock 1970, 295 cited in Bradley *et al.* 1989). It was known locally as the 'Black Abbey' because of the black habit worn by the friars (<http://www.limerickdioceseheritage.org/Adare/chAdareAug.htm>). In 1464 his descendant, Thomas Fitzgerald 7th Earl of Kildare, established a Franciscan Friary dedicated to St Michael the Archangel on the north bank. All three religious houses in Adare were suppressed in 1539–40, though the friars appear to have remained until c. 1580 (Bradley *et al.* 1989, 18 & 32). The Franciscan Friary and Trinitarian Abbey were granted to Sir Henry Wallop in 1595 (*ibid.*). Restored in the 19th century, the Augustinian Friary now houses the Church

of Ireland parish church and national school while the Trinitarian abbey was rebuilt to serve as the Roman Catholic parish church (<http://www.limerickdioceseheritage.org/Adare/chAdareAug.htm>).

Although clearly a substantial settlement in the 13th and 14th centuries, Adare appears to have entered a period of slow decline following the burning of the town in 1376 (Bradley *et al.* 1989, 1). By the late-17th century the old Fitzgerald estates, centred on Adare, had passed to Mr. Thady Quin whose descendents became the Earls of Dunraven (Wyndham-Quin and Wyndham-Quin 1865, 146). Adare was revived in the 19th century by the second Earl Wyndham-Quin who effectively rebuilt the town and many of its architectural attractions date to this period and contribute much to its present character (O'Connor 1987, 90).

This rebuilding, however, altered the layout of the town substantially and removed any evidence that may have remained of the medieval property boundaries and town walls (Bradley *et al.* 1989, 2–3). The clustering of Desmond Castle, the parish church and the Franciscan Friary to the north of the Maigue suggest that this was the site of the main medieval settlement. However, the Down Survey map of c. 1656 depicts the town as being to the south of the river (O'Connor 1987, 26). Recent research has identified a tentative line of the town wall, to the south of the Maigue and encompassing the Trinitarian Abbey and Augustinian Friary (Sarah McCutcheon pers. comm.).

6.2. Archaeological heritage

There are 14 recorded monuments within 500 m of the proposed scheme (Fig. 1; Table 1). Most notably, the northern end of the scheme is within the zone of archaeological potential of the historic town (LI021-032001) as outlined on the RMP map and also crosses the tentative line of the town wall (Fig. 2; Plate 1). In addition, the site of the proposed car park on the Rathkeale Road, which encompasses c. 0.1024 ha of greenfield land, is considered to be of archaeological potential due to its proximity to the historic town and to a former fever hospital (Plates 2 and 3).

Other important monuments located within the town, but more than 500 m from the scheme, include the Augustinian Friary (LI021-032006), Desmond Castle (LI021-032003; National Monument no. 622), the medieval parish church (LI021-032004) and the Franciscan Friary (LI021-032007).

Table 1 – Recorded monuments within 500 m of the proposed scheme

Townland	Site Type	Legal Status	RMP No.	Dist. from Scheme (m)
Adare, Blackabbey, Ardshanbally, Gortaganniff	Historic Town - Adare	RMP	LI021-032001	0
Graigue (Coshma By.)	Enclosure	RMP	LI021-156001	50
Graigue (Coshma By.)	Enclosure	RMP	LI021-156003	112
Graigue (Coshma By.)	Enclosure	RMP	LI021-156002	123
Cummeen	Enclosure	RMP	LI021-157001	141
Cummeen	Enclosure	RMP	LI021-157003	165
Cummeen	Enclosure	RMP	LI021-157002	192
Blackabbey	Dovecote	RMP	LI021-032012	265

Townland	Site Type	Legal Status	RMP No.	Dist. from Scheme (m)
Blackabbey	Religious house - Trinitarians	RMP	LI021-032008	285
Blackabbey	Burial	RMP	LI021-032019	300
Blackabbey	Redundant record	RMP	LI021-154	380
Blackabbey	Redundant record	RMP	LI021-155	342
Cummeen	Enclosure	RMP	LI021-119	455
Islandea	Redundant record	RMP	LI021-152	482

6.3. Previous archaeological investigations

Seven archaeological investigations in Adare town centre have resulted in the discovery of archaeological objects and deposits (Table 2). These include pottery and probable domestic waste of both medieval and post-medieval origin, as well as possible human bone, stone culverts and old road surfaces. Despite being limited in extent, these investigations indicate that medieval and post-medieval remains survive below the modern ground surface within the town.

Of particular note are the excavations carried out by McCutcheon in advance of the Adare Main Street Improvements Scheme. The extent and scope of this project was very similar to the works proposed under the present scheme which, though further from the medieval core of the town, may nevertheless be expected to reveal sub-surface archaeological deposits.

Table 2 – Summary results of previous archaeological investigations in Adare Town

Location	ITM Co-ords	Licence No.	Director	Description	Excavations no.
Main Street	546062, 646046	96E0375	Una Cosgrave	Test excavations revealed a black organic layer 0.35m deep, modern foundations, a truncated, possibly post-medieval linear feature and sherds of possibly post-medieval pottery and a small quantity of animal bone.	1996:229
Adare Manor	546762, 646146	N/A	Celie O Rahilly	Unlicensed monitoring of two percolation tests in the grounds of Adare Manor revealed medieval pottery and a possible pit or ditch, as well as a deposit of possible ash, charcoal, animal bone and some stones.	1998:375
Adare Manor Golf Club	546962, 646946	N/A	Celie O Rahilly	Extension of the clubhouse was carried out without archaeological supervision. A site inspection noted sherds of medieval pottery in the spoil heap.	1998:374

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Location	ITM Co-ords	Licence No.	Director	Description	Excavations no.
Main Street	546864, 646491	99E0084	Sarah McCutcheon	Archaeological monitoring was carried out on the Adare Main Street Improvements Scheme which entailed pipe-laying and upgrading the road surface over a length of 480m from the Dunraven Arms Hotel to just beyond the junction with the Askeaton road. Towards the eastern end of the village, pits and a medieval stone culvert were excavated, as well as layers of old road surface made up of levelled-out bedrock, metalling, rough cobbling and a more formal stone surface. An earlier, similar stone surface was recorded beneath, but this was protected and remains undisturbed. In the western portion, post-medieval/modern material was recorded.	1999: 475
Blackabbey	546762, 646546	04E1581	Mary Henry	Pre-construction test trenching at St Nicholas National School revealed post-medieval rubbish pits and a stone culvert of possible 19th-century date.	2004:0968
Islandea and Blackabbey	545615, 647251	04E1131	Margaret McCarthy	Three possible enclosures—LI021-152, LI021-154 and LI021-155—were tested in advance of a housing development. No evidence for the enclosures was recorded but a stone axe was found near LI021-154.	2004:0970
Blackabbey	546774, 646578	12E030	Frank Coyne	Test trenches at the site of St. Nicholas's church confirmed that the area around the church has been built up with hardcore and gravel to a depth of 0.4 m, below which is an old topsoil layer containing probable human bone.	2012:378

6.4. Architectural heritage

As the proposed scheme is confined to the existing road footprint and footpaths, there are no architectural heritage features within the scheme boundary. However, the scheme is located partly within the Architectural Conservation Area for the town, as outlined on Map No. 4 of the Adare Local

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Area Plan (Limerick City and County Council 2015). Seven main areas of interest are identified within the ACA, of which ACA.6 'Clifford Smith Village Hall and Housing' and ACA.7 'The Avenue' are adjacent to the present scheme (*ibid.*, 52).

There are 27 buildings/structures of architectural heritage value in close proximity to the scheme, each of which has been assigned a unique Built Heritage number for the purposes of this study (Figs 3a and 3b; Table 3; Plates 4–11). One of these buildings, a former fever hospital (BH22), is located c. 43 m from the site of the proposed car park on the Rathkeale Road.

All of the NIAH buildings listed in Table 3 are of Regional importance, with the exception of BH02, BH17 and BH26 which the NIAH has rated as being of Local importance.

Table 3 – Buildings/structures of architectural heritage value in the vicinity of the scheme, identified from the RPS and the NIAH

Built Heritage no.	Location	RPS Reg. & Ref. nos	NIAH Reg. No.	Description
BH01	Adare Town	MT03	-	Adare Town—medieval township and Architectural Conservation Area
BH02	Junction of Main Street and Blackabbey Rd.	-	21824037	'Bia/Adare Hair Studio'—attached two-bay two-storey shop, built c. 1820, formerly in use as house
BH03	Junction of Main Street and Rathkeale and Blackabbey roads	879/ M21M(9)	21824007	Community Hall in the 'Arts and Crafts' style, built in 1911
BH04	Rathkeale Rd (west side)	885/ ADR-39(ii)	21824008	'Liston's'—terraced two-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH05	Rathkeale Rd (west side)	886/ ADR-39(iii)	21824009	'Judge's'—end-of-terrace three-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH06	Rathkeale Rd (west side)	887/ ADR-39(iv)	21824010	'Savage's'—end-of-terrace two-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH07	Rathkeale Rd (west side)	888/ ADR-39(v)	21824011	'O'Leary's'—terraced two-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH08	Rathkeale Rd (west side)	889/ ADR-39(vi)	-	'Walsh's'—terraced three-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH09	Rathkeale Rd (west side)	890/ P.P.S.290	21824012	'Iona'—end-of-terrace three-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH10	Rathkeale Rd (west side)	891/ ADR-39(vii)	-	'Jones's'—end-of-terrace three-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH11	Rathkeale Rd (west side)	892/ M21M(10)	21824013	'Kilmurray's'—terraced three-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith

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Built Heritage no.	Location	RPS Reg. & Ref. nos	NIAH Reg. No.	Description
BH12	Rathkeale Rd (west side)	893/P.P.S. 291	-	'Iverk'—terraced two-bay single-storey with dormer attic house, built c. 1910. Part of house terrace designed by William Clifford-Smith
BH13	Corner of Main St. and Rathkeale Rd.	-	21824032	'Pat Collins Bar'—corner-sited attached three-bay two-storey house and public house, built c. 1830
BH14	Rathkeale Rd (east side)	894/M21M(11)	21824031	'Raincín'—commercial premises. Attached single-bay two-storey house and shop, built c. 1820
BH15	Rathkeale Rd (east side)	895/ADR-41	21824030	'Chawke's Bar'—commercial premises. Attached four-bay two-storey house and public house, built c. 1820
BH16	Rathkeale Rd (east side)	896/M21M(13)	21824029	Detached gable-fronted single-bay single-storey former forge, built c. 1820
BH17	Rathkeale Rd (east side)	898/ADR-43(ii)	21824028	Detached three-bay single-storey former coach house, built c. 1850
BH18	Rathkeale Rd (east side)	897/ADR-43(i)	21824027	Detached four-bay two-storey former constabulary barracks, built c. 1820
BH19	Back Avenue (south side)	1681/ADR-55	-	Semi-detached gabled, two bay, dormer-roofed cottage
BH20	Rathkeale Rd (east side)	1688/ADR-58	-	'St. Gerard's'—detached two-storey house built in the late 1920s
BH21	Rathkeale Rd (east side)	1689/ADR-59	21824016	Dovea House/'The Doctor's'—detached four-bay two-storey house, built c. 1910
BH22	Rathkeale Rd (east side)	1687/M21M(12)	21824014	Detached three-bay two-storey former fever hospital, built in 1830, now a school
BH23	Rathkeale Rd (east side)	826/PPS 311	21824084	Detached two-bay two-storey former gate lodge, built c. 1830. Described in RPS as an estate cottage
BH24	Rathkeale Rd (east side)	1690/M(20)(i)	-	Deerpark Gateway—country house feature
BH25	Rathkeale Rd (west side)	1691/M(20)(ii)	21824015	'Davy's or Deerpark Lodge—detached two-bay two-storey former gate lodge, built c. 1820
BH26	Killarney Rd. (east side)	1692/ADR-62	21902108	'Ivy House'—detached three-bay two-storey house, built c. 1830
BH27	Rathkeale Rd. (east side)	-	-	Stone gate piers and wrought iron gate. Directly opposite BH24. Of probable 19th century date.

7. Recommendations

7.1. Archaeological heritage

7.1.1 The proposed road scheme is located partly within the zone of archaeological potential of the historic town of Adare (LI021-032001), as depicted on the RMP map. The area of the historic town within this zone constitutes a Recorded Monument and is afforded statutory protection under Section 12 of the National Monuments Act 1930–2004. Previous investigations in the town have clearly demonstrated that medieval and post-medieval remains are present below the modern ground surface. These remains are likely to be disturbed by the construction works envisaged as part of the present scheme.

In addition, the scheme crosses the tentative line of the town wall near the junction of the Rathkeale Road and Main Street (within the zone of archaeological potential). Although no evidence of town defences have previously been discovered in Adare, there is documentary evidence that the town was walled in the 14th century. If archaeological remains of the town wall are discovered during construction, those remains will constitute a National Monument and it will be necessary to obtain Ministerial Consent, in accordance with Section 14 of the National Monuments Act, before any works can be carried out on, or in the vicinity of the monument (DoEHLG 2008, 4).

It is recommended, therefore, that targeted archaeological test excavations be carried out in advance of construction, if feasible, at the point where the scheme crosses the line of the town wall in order to determine the presence or otherwise of the town defences within the scheme footprint.

It is further recommended that all construction works within the zone of archaeological potential of the historic town be archaeologically monitored. The portion of the scheme to be monitored extends south/south-west from the junction of the N21 Rathkeale Road and the L1422 Blackabbey Road for a distance of approximately 170 m, to just beyond the junction of the N21 Rathkeale Road and the Avenue.

7.1.2 Due to its proximity to the historic town and to a former fever hospital, the site of the proposed car park on the Rathkeale Road is considered to be of archaeological potential. It is recommended that archaeological test excavations be carried out at this location in advance of construction to determine the presence or otherwise of any archaeological remains.

7.1.3 All archaeological test excavations and/or monitoring should be carried out under licence by a competent archaeologist. Any archaeological remains uncovered during such investigations will require either preservation *in situ* or preservation by record (i.e. archaeological excavation and recording), as appropriate.

7.2. Architectural heritage

The proposed scheme does not impact directly on any buildings/structures of architectural heritage value. However, as the scheme is partly within the ACA for Adare Town, it is recommended that the design team consult with Mr. Tom Cassidy, Limerick City and County Council Conservation Officer, to ensure that the proposed works comply with the objectives of the county development plan to preserve the unique character of the area.

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www.logainm.ie

www.limerickdioceseheritage.org

Plates



Plate 1: Northern portion of the scheme located within the zone of archaeological potential of the historic town. Looking NNE from the Avenue towards the Community Hall (BH03)



Plate 2: Boundary wall of site of proposed car park on the western side of the Rathkeale Road. Looking north



Plate 3: Site of proposed car park, looking east towards Rathkeale Road



Plate 4: Community Hall (BH03) at the junction of Main Street, the N21 Rathkeale Road (at left) and the L1422 Blackabbey Road (at right)



Plate 5: 'Liston's' (BH04) at centre and 'Judge's' (BH05) at left, part of house terrace designed by William Clifford-Smith



Plate 6: 'Iona' (BH09), part of house terrace designed by William Clifford-Smith



Plate 7: Former forge (BH16), built c. 1820



Plate 8: Front (western) elevation of former fever hospital (BH22)



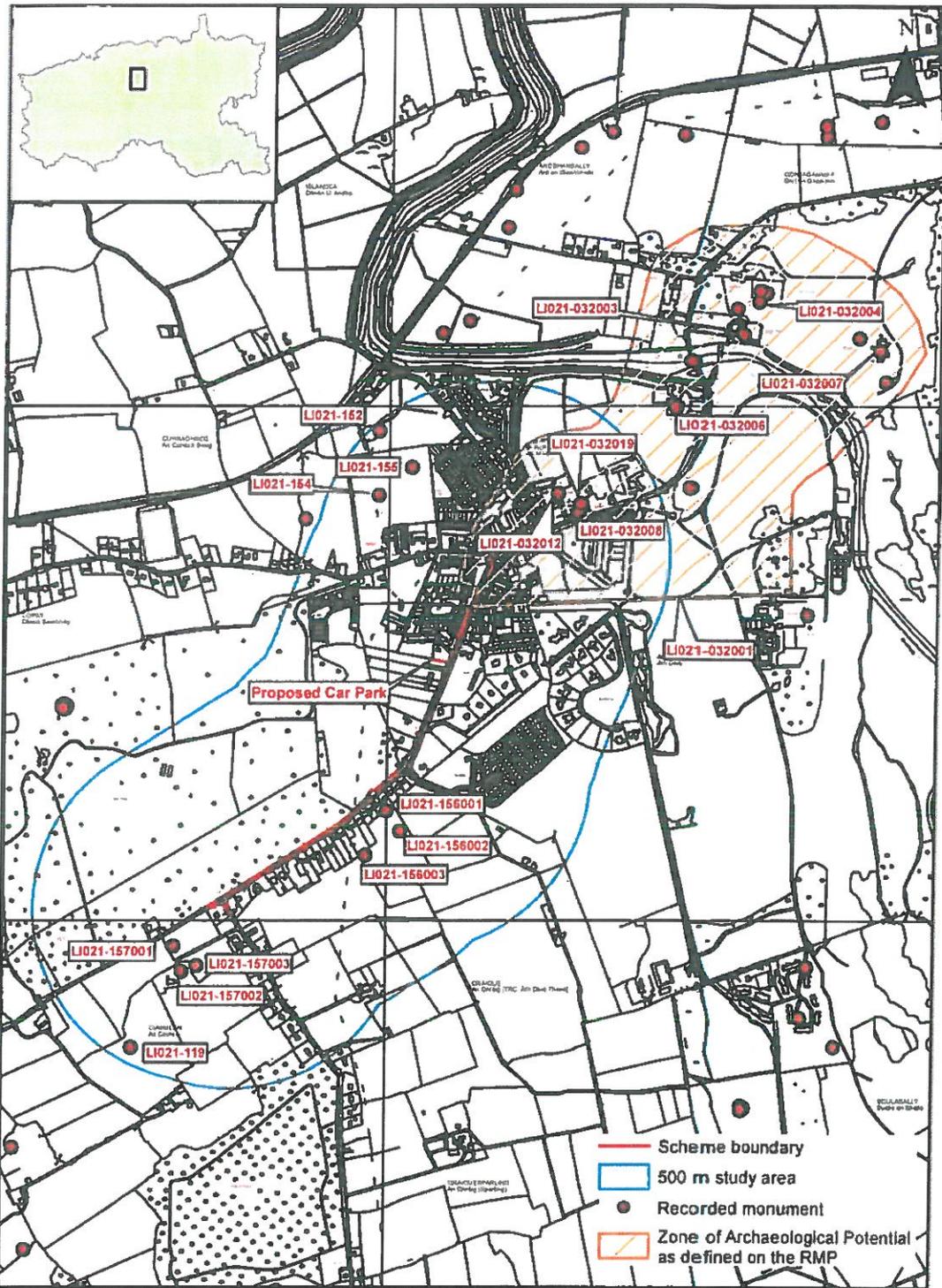
Plate 9: Side (southern) elevation of former fever hospital (BH22)



Plate 10: Deerpark Gateway (BH24) and gate lodge (BH23), looking south-west

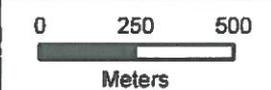


Plate 11: Gateway (BH27) on the eastern side of the Rathkeale Road, opposite BH24



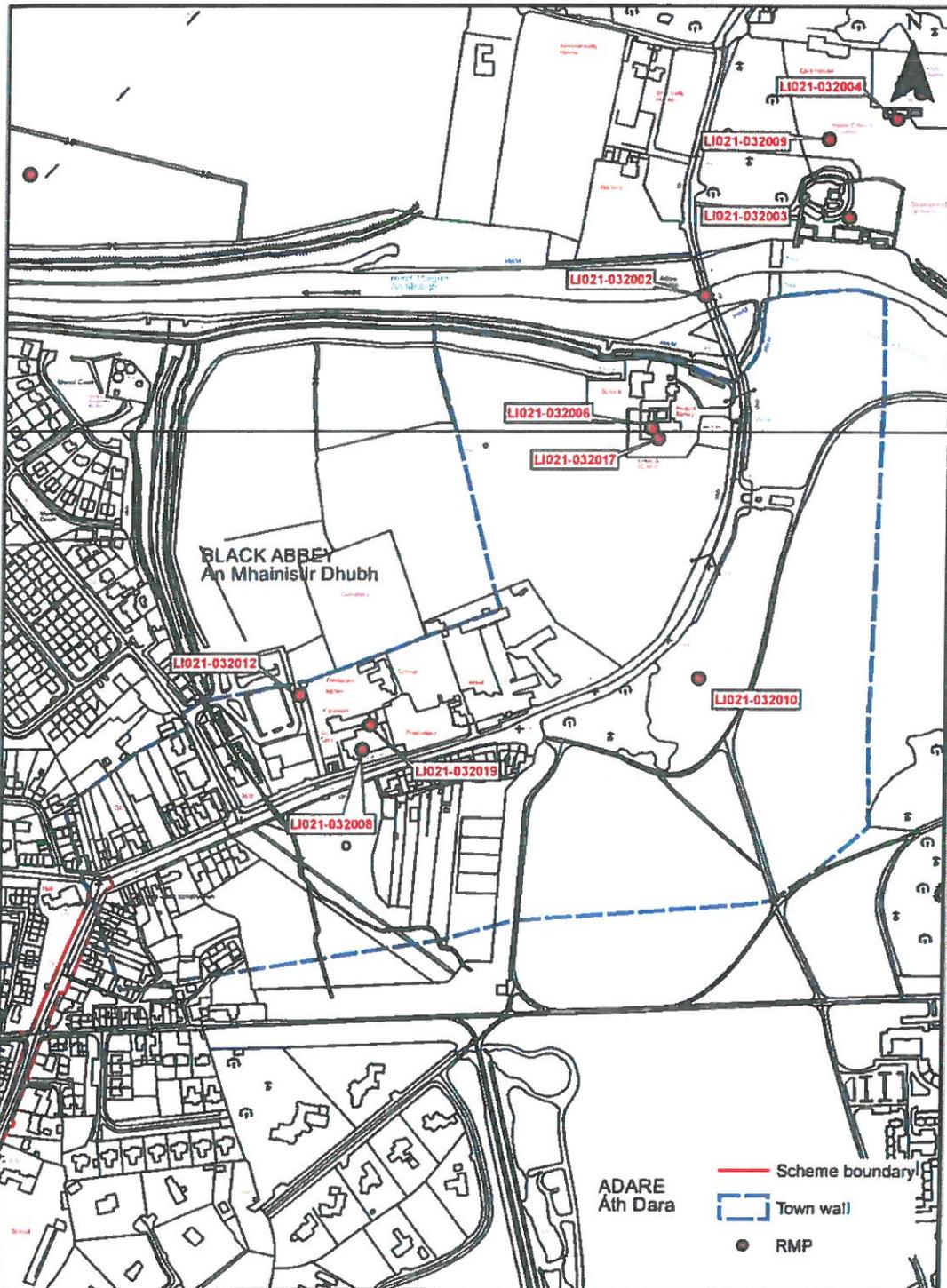
N21 Adare Western Approach Improvements
Figure 1: Recorded monuments in the vicinity of the scheme

Scale: 1:18000 @ A4
 Prepared by: Paul O'Keeffe
 Date: 10/03/16



Background: OSi 1:2500 and 1:5000 vector maps © Ordnance Survey Ireland, Government of Ireland. Licence no. EN0045214.





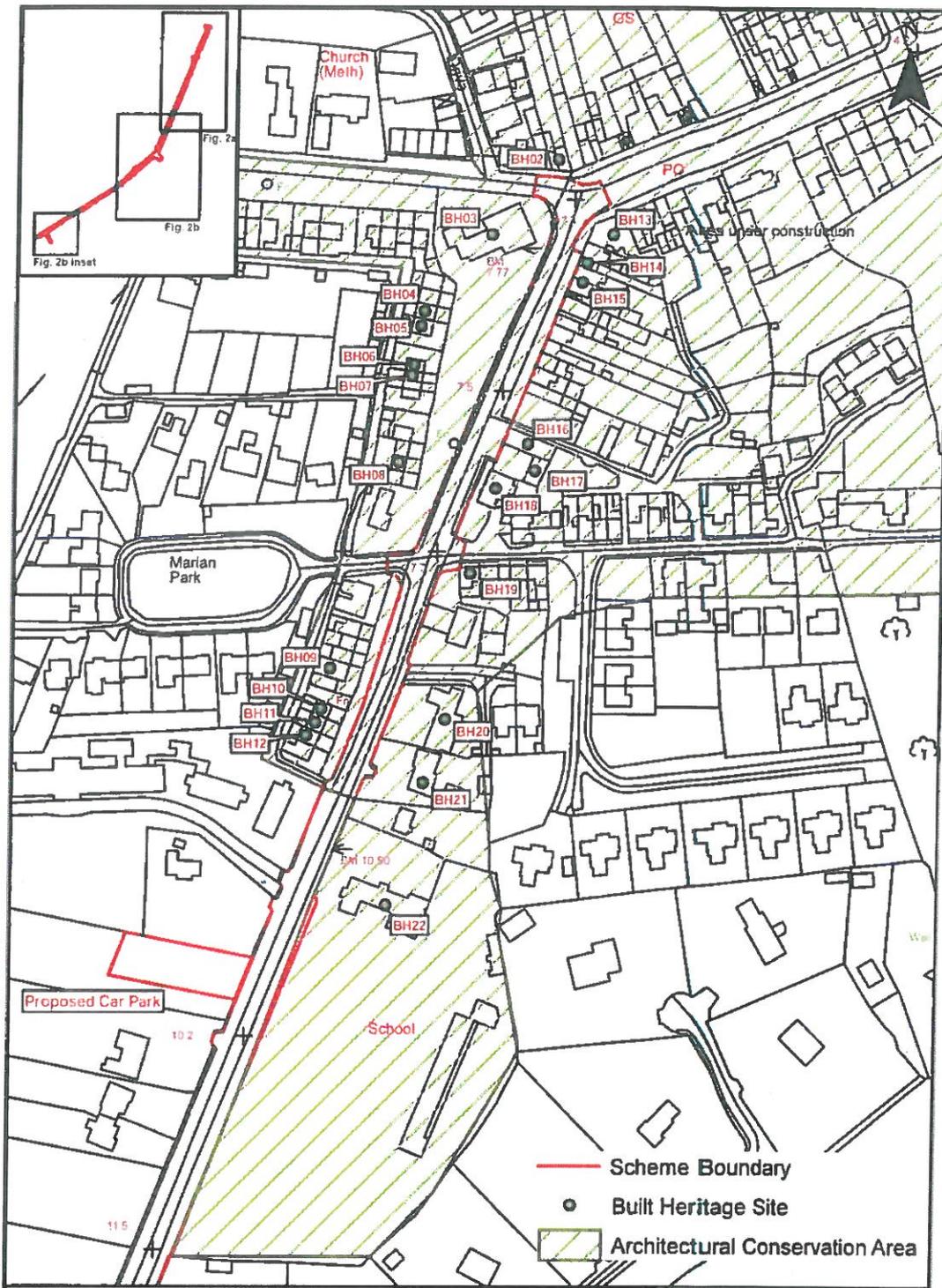
N21 Adare Western Approach Improvements
Figure 2: Tentative line of the town wall (after McCutcheon, unpublished)

Scale: 1:6000 @ A4
 Prepared by: Paul O'Keeffe
 Date: 30/10/15



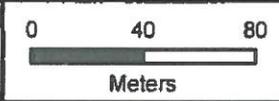
Background: OSI 1:2500 and
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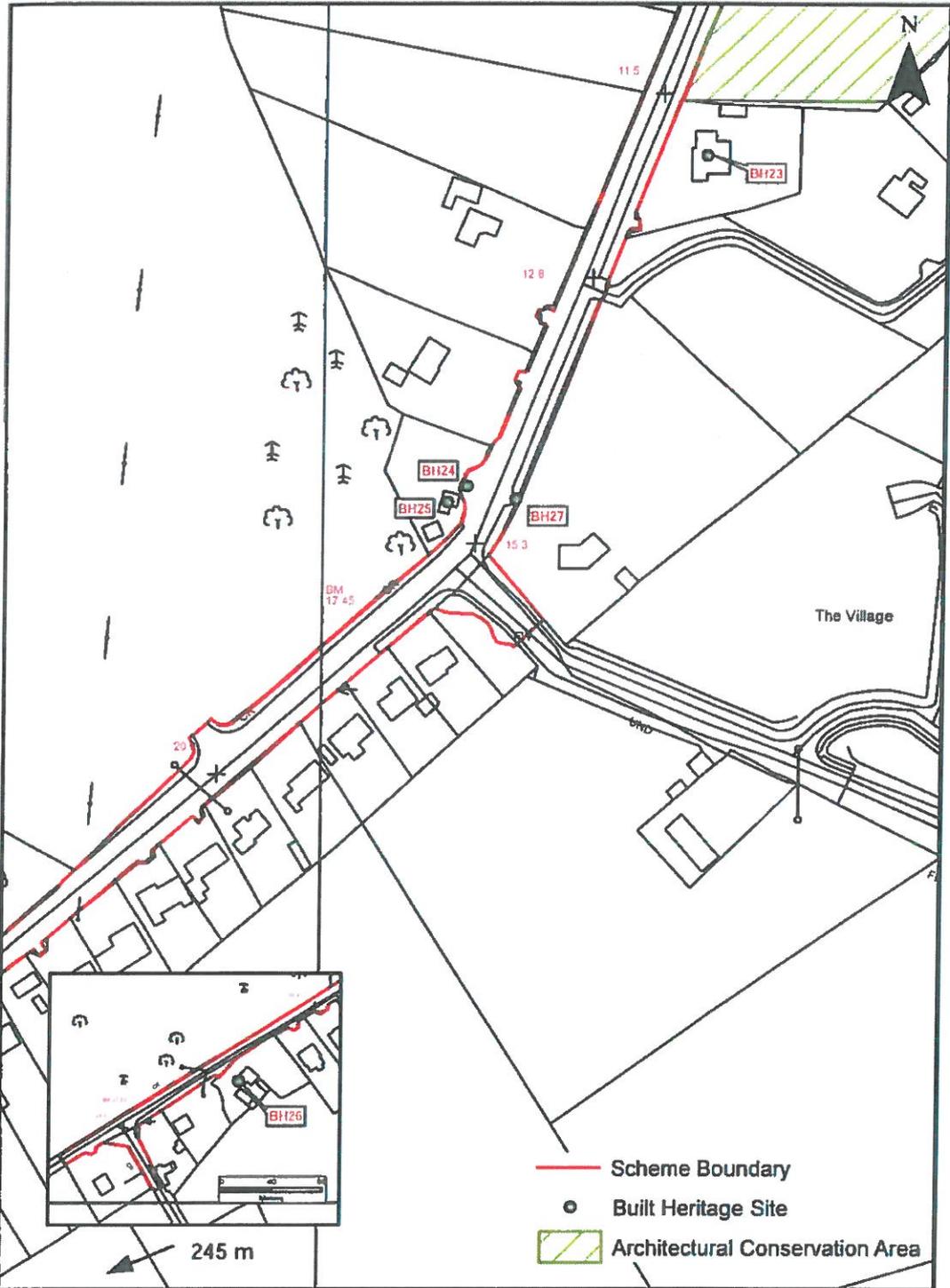
N21 Adare Western Approach Improvements
Figure 3a: Architectural heritage in the vicinity of the northern portion of the scheme

Scale: 1:2500 @ A4
 Prepared by: Paul O'Keeffe
 Date: 10/03/16



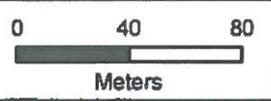
Background: OSI 1:2500 and 1:5000 vector maps © Ordnance Survey Ireland, Government of Ireland. Licence no. EN0045214.





N21 Adare Western Approach Improvements
Figure 3b: Architectural heritage in the vicinity of the southern portion of the scheme

Scale: 1:2500 @ A4
 Prepared by: Paul O'Keeffe
 Date: 10/03/16



Background: OSi 1:2500 and 1:5000 vector maps © Ordnance Survey Ireland, Government of Ireland. Licence no. EN0045214.



APPENDIX 2
Submissions Received

Submission 1

14 Penrose Wharf, Cork, Ireland

t. +353 21 486 1385

f. +353 21 486 1386

e. info@kpm.ie w. www.kpm.ie



KEENAN

Property Management
your property • our priority

Mr Seamus Linehan
Roads Engineer
Limerick City and County Council
Mid West National Road Design Office
Lissnalta House
Dooradoyle
Limerick

23 March 2016



Ref The Village, Adare Manor, Co Limerick

Dear Seamus,

Thank you for your telephone call today. As you know we are the managing agents for The Village, Adare Manor.

With the new N21 overlay of road from Adare town to Murphy's Cross – we understand that there is no "yellow box" junction on these plans and we would like to raise this as a query with you.

A number of residents have brought up the need for a "Yellow Box" at the junction for traffic coming from the west. Very often when traffic flow is busy, traffic backs up – blocking the junction and preventing traffic from exiting The Village or manor fields, on a green light. With the planned development of 40 houses at Graigue Adare (planning ref 15978) and the additional traffic during the construction – this yellow box becomes a necessity so that the traffic exiting from the Village, Manor Fields and new development, can do so safely.

We look forward to hearing from you.

Yours sincerely,

Catherine Dunford
Keenan Property Management
On behalf of Adare Village Owners Management Co Ltd

Head Office, Unit 13-14 Woodford Court, Woodford Business Park, Santry, Dublin 17, Ireland
t. +353 1 844 2400 f. +353 1 844 2450 e. info@kpm.ie w. www.kpm.ie

Directors Brian Keenan B.Sc., Dip.Prop.Econs. MIAVI Niamh Shannon, B.Sc., Dip.Prop.Econs
Michelle Austin, B.Sc., Prop. Valuations and Mgmt. Andrew Weir, B.Sc. (Hons) Estate Mgmt

Registered in Dublin No 270243 VAT No 8270743H

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MyHome.ie

Submission 2

From: [Mark Leahy](mailto:Mark.Leahy@midwestroads.ie)
To: slinehan@midwestroads.ie
Cc: tfitzgerald@midwestroads.ie; [Ger.Carey \(gcarey@midwestroads.ie\)](mailto:Ger.Carey@midwestroads.ie)
Subject: FW: Car Park & Retirement Village - Adare Co. Limerick
Date: 24 March 2016 10:33:29
Attachments: [160322 - ADARE PROPOSAL.PDF](#)

Gents-See attached for information. Ye might let me know what your views/comments are on this so we can formulate a response back to Mary O'Malley.

Mark

From: O'Malley Mary [<mailto:mary.omalley@limerick.ie>]
Sent: 23 March 2016 14:46
To: Leahy Mark
Subject: Fw: Car Park & Retirement Village - Adare Co. Limerick

Hi Mark

I have attached a preplanning proposal that we have received. The applicant is looking to access his lands through the carpark proposed in the Part 8 improvement scheme. Could you review the proposal and send on any comments. Our preference in planning would be one access off the N21 (as shown in the Adare Local Area Plan) that then spurs to the proposed development site on one side and the car park on the other and in the future provide access to the other lands to the rear.

Regards,

Mary

From: O'Donoghue Donogh
Sent: 23 March 2016 14:25
To: O'Malley Mary
Subject: Fw: Car Park & Retirement Village - Adare Co. Limerick

From: Shane Lahiff <Shane.Lahiff@fhp-architects.com>
Sent: 23 March 2016 09:51
To: O'Donoghue Donogh
Subject: Car Park & Retirement Village - Adare Co. Limerick

Dear Donogh,

My name is Shane Lahiff and I am an Architect based in Waterford with Fewer Harrington & Partners and one of our clients has asked us to look into the feasibility of a proposal for Adare.

The site is positioned directly across from Scoil Naomh Iosaf on the main Limerick to Tralee Road. The existing dwelling to the front of the proposed site facing the main road, is my client's family

home. There is a current car park proposal published in the 'N21 Western Approach Improvement Scheme' on the adjacent site which has been undertaken by Roadplan . (See attached)

Our client's wishes to propose an amendment to the proposed council car park layout that whilst providing the same number of car park spaces, also provides a clear new road access that can both service our client's lands and the lands in ownership by the council to the rear of the car park for future development.

I have attached a pdf outlining both Roadplan's proposal and our client's intentions which are in line with Roadplan's proposal and would fulfil the Council's criteria for car parking on their site. The introduction of the road will also create a safer car parking layout and would also provide access to council lands to the rear should future development be proposed.

If the council are in agreement over the construction of a new road it would be our client's intention to create a aged / retirement housing scheme which will incorporate 8 no. dwellings and a 2-3 storey apartment building which my client proposes to retire/ reside within the third floor apartment, once the development has been completed.

I am contacting you to seek guidance on how to proceed with the proposed development as the proposal includes infrastructural changes which may need to be overseen by council roads engineers or if you feel a pre planning meeting in relation to the proposal would be more beneficial please let me know. Our client is anxious to proceed with the above development on his lands and to engage accordingly.

if you would like to contact me in relation to the proposal or have any questions please contact me on 051-876 991.

Regards

Shane

E: shane.laluff@fhq.architects.com

W: www.fhq.architects.com

FEWER HARRINGTON & PARTNERS

ARCHITECTS, PLANNERS & PROJECT MANAGERS

Waterford Office

Studio 14, The Atrium, Marlana Gate, Canada Street, Waterford.

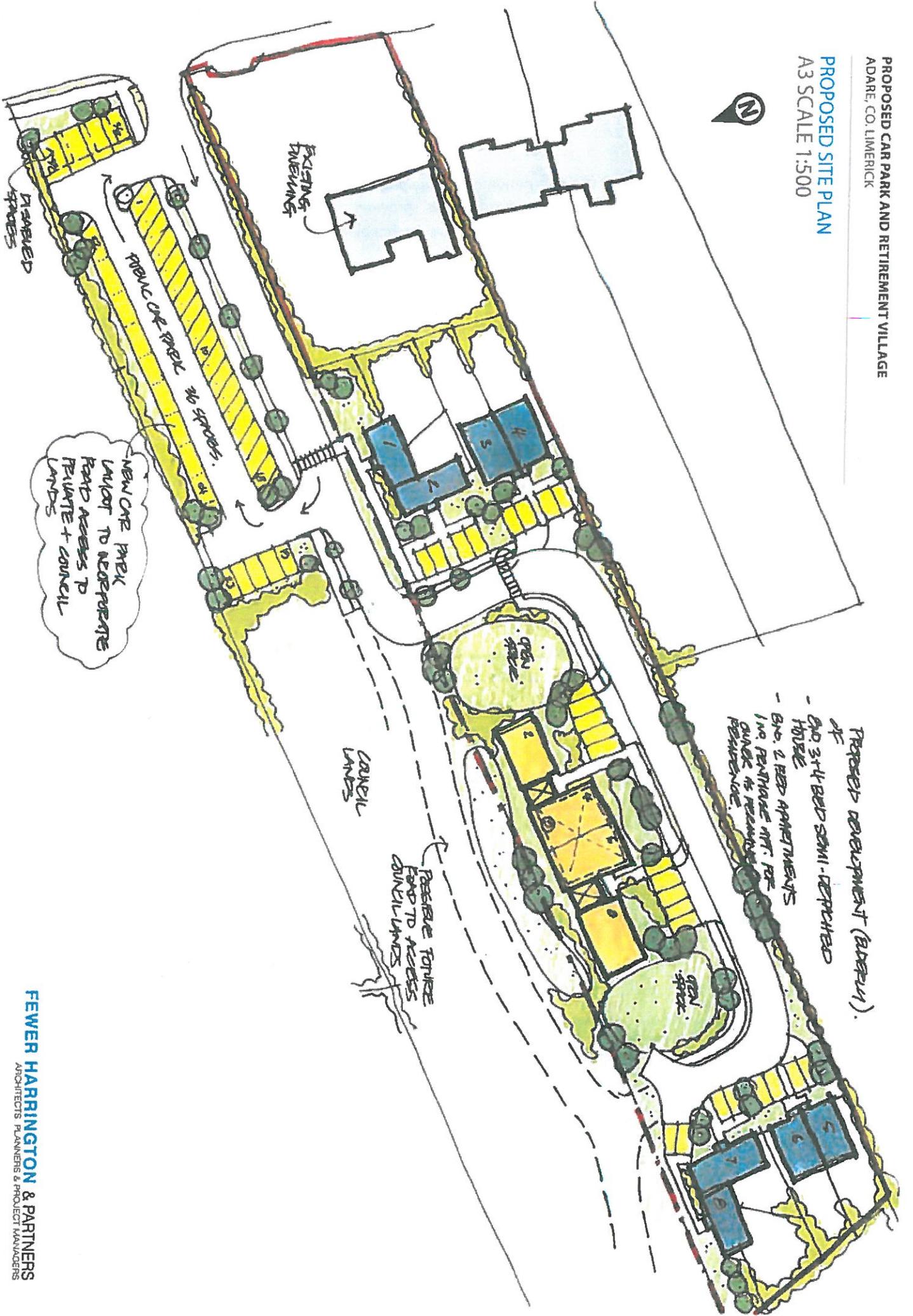
Phone (+353) 51 876 991 : FAX (+353) 51 878 676 : Email waterford@fhq.architects.com

with offices in Waterford, Dublin, Jeddah & Riyadh

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PROPOSED SITE PLAN
A3 SCALE 1:500



Proposed development (blueprint).
- 2nd 3+4 BED SEMI-DETACHED HOUSE
- 2nd 2 BED APARTMENTS
- 1st 2 BED APARTMENTS
- 1st 2 BED APARTMENTS
- 1st 2 BED APARTMENTS

NEW ONE PAVED LAYOUT TO REDEVELOPE ROAD ACCESS TO PRIVATE + COUNCIL LANDS

CORNER LANDS
RESERVE FENCE ROAD TO ACCESS COUNCIL LANDS

Submission 3

From: [Tim Fitzgerald](mailto:Tim.Fitzgerald)
To: [Ger Carev \(gcarev@midwestroads.ie\)](mailto:Ger.Carev@midwestroads.ie); [Seamus Linehan](mailto:Seamus.Linehan)
Subject: FW: Adare Road Works
Date: 03 May 2016 08:53:32

For Attention

From: Kennedy Brian [mailto:brian.kennedy@limerick.ie]
Sent: 02 May 2016 18:01
To: Fitzgerald Tim
Cc: Noonan Roger; Flanagan Declan; Curley Caroline
Subject: Fwd: Adare Road Works

Hi Tim, please see email from Adare tidy towns.

Sent from my iPad

Begin forwarded message:

From: Adare Tidy Towns <adaretidytowns@gmail.com>
Date: 2 May 2016 10:20:59 IST
To: brian.kennedy@limerick.ie
Subject: Adare Road Works

Dear Brian

Following a meeting of the Community Council and local councillors in Adare last Tuesday April 26 Adare Tidy Towns requests that as much work as is possible would be conducted at night to minimize the disruption to traffic flow.

Yours

Eleanor Purcell
Secretary

LimerickOne of the 'TOP 10 PLACES TO INVEST IN EUROPE'- Site Selection Magazine, 2014. Limerick City & County Council Disclaimer: This electronic message contains information (and may contain files), which may be privileged or confidential. The information is intended to be for the sole use of the individual(s) or entity named above. If you are not the intended recipient be aware that any disclosure, copying, distribution or use of the contents of this information and or files is prohibited. If you have received this electronic message in error, please notify the sender immediately. This is also to certify that this mail has been scanned for viruses. Tá eolas sa teachtaireacht leictreonach seo (agus b'fhéidir sa chomhaid ceangailte leis) a d'fhéadfadh bheith príobháideach nó faoi rún. Is le h-aghaidh an duine/na ndaoine nó le h-aghaidh an aonáin atá ainmnithe thuas agus le haghaidh an duine/na ndaoine sin amháin atá an t-eolas. Murab ionann tusa agus an té a bhfuil an teachtaireacht ceaptha dó bíodh a fhios agat nach gceadaítear nochtadh, cóipeáil, scaipeadh nó úsáid an eolais agus/nó an chomhaid seo. Más trí earráid a fuair tú an teachtaireacht leictreonach seo cuir, más é do thoil é, an té ar sheol an teachtaireacht ar an eolas láithreach. Deimhnítear leis seo freisin nár aimseodh víreas sa phost seo tar éis a scanadh. 

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Submission 4



Killeheen,
Reens,
Ardagh,
Co. Limerick.
11/05/2016

Dear Seamus;

I hope this letter finds you well. I am attaching with this letter a submission I wish to make on behalf of some of the residents in Adare in relation to the proposed Adare Western approach scheme.

I hope you find the attached to be in order and I would ask for you to give it strong consideration. I would be grateful for anything you can do in relation to this matter.

Kind Regards,

A handwritten signature in blue ink that reads 'Adam Teskey'. The signature is written over a solid black horizontal line.

Cllr. Adam Teskey

To Whom it concerns;

We the attached wish to make a submission with regard to the N21 Adare Western approach scheme.

We are asking for the above mentioned scheme to incorporate a Yellow box directly outside Patrick Collins bar to serve the people who commute and access the Black Abbey road from the N21.

This will prevent traffic from building up from the Limerick side of Adare who wish to use the Black abbey road. At present motorists who use the turning in slip Lane are prohibited from accessing the black abbey road if there is a build-up of traffic from the Rathkeale side of Adare which is a regular occurrence.

Also if this yellow box is put in place it will also help motorists who wish to turn right for Rathkeale off the Black Abbey road as when there is a build-up of traffic on the N21 commuters wishing to turn for Rathkeale are also prohibited from doing so.

We hope that you will consider our submission and we look forward to seeing progress being made with regard to same.

Please find attached Names and address of the relevant residents who wish to make this submission.

Name.

Ernie Walsh

Betty Walsh

Peter Walsh

Bridie Riordan

Samuel I. Shum

Address.

Embury Close Adare

Embury Close Adare

Truoh Adare G. Limerick

Adare

Adare

Submission 5

From: [Helen Clery](#)
To: [Seamus Linehan](#); [Clara Power](#)
Subject: FW: N21 Adare Western Approach Improvement Scheme
Date: 10 May 2016 15:37:25

From: Scoil Naomh Iosaf Adare [<mailto:sniadare.ias@gmail.com>]
Sent: 10 May 2016 15:30
To: info@midwestroads.ie
Subject: N21 Adare Western Approach Improvement Scheme

To whom it may concern:

After closely examining the part 8 drawings for the Adare Western Approach Improvement Scheme, I wholly agree and approve of the plans. The proposals to add a car park across the road and to widen the existing footpath and lighting area would be a huge improvement to the Health and Safety standards currently existing outside the school. These plans would provide great relief to us by ensuring safer access to and from the school.

If I was to make one suggestion, it would be the placement of a speed vehicle actuated sign at the red surfacing slow sign on the road. These are a great deterrent to speeding vehicles and would be a further improvement on the existing plans.

Kind regards,

Stephen Lavin,
Principal

Scoil Naomh Íosaf,
Adare,
Co. Limerick

Tel: 061 396101
Email: info@scoilnaomhiosafadare.com
Web: www.scoilnaomhiosafadare.com

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Submission 6

From: [Helen Clery](#)
To: [Seamus Linehan](#); [Ciara Power](#)
Subject: FW: N21 Adare Western Approach Improvement
Date: 11 May 2016 09:11:54

From: Karina Tarrant [<mailto:karina.tarrant@gmail.com>]
Sent: 10 May 2016 19:26
To: info@midwestroads.ie
Subject: N21 Adare Western Approach Improvement

To whom it may concern :

My name is Karina Byrne. I am chairperson of Scoil Naomh Iosaf's Parent's Council in Adare. On behalf of the parents of the school I would like to let you know that we support the proposed changes to the N21 in light of health and safety and feel it would provide more adequate parking space.

Regards,

Karina Byrne
Chairperson
Parent's Council
Scoil Naomh Iosaf

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Submission 7

From: Helen Clery
To: Clara Power; Seamus Linehan
Subject: FW: N21 upgrades Adare limerick.
Date: 12 May 2016 10:58:54
Attachments: DSC_0546.jpg

From: sean collins [mailto:sean.collinsbar3@gmail.com]
Sent: 12 May 2016 10:53
To: info@midwestroads.ie
Subject: N21 upgrades Adare limerick.

Sean Collins Adare here, in front of my bar could you put an extend area of footpath taking up the space of a car space, it would greatly help when existing the laneway and turning left or right. There are about 20 cars using the lane every day with about 80 trips up and down. Can the new carpark be open all year also. I know that loosing a car space is not good but if someone had a crash it would be a lot worse thank you Sean Collins.

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Submission 8

Planning Department,
Limerick City and County Council,
7/8 Patrick Street,
Limerick.

12th May 2016

Submission on N21 Adare Western Approach Pavement Improvement Scheme

Dear Sir or Madam,

The Adare Community Council in conjunction with the other organisations within the village, is fully in favour of the above improvement works going ahead. This is going to be a major project on a very busy stretch of road, and will seriously affect traffic flow in both directions.

We urge Transport Infrastructure Ireland (TII) to have alternative routes well marked and signposted as far back as Newcastle West as well as from Limerick City, the M7 and M18 Motorways plus the N20.

We believe that Transport Infrastructure Ireland has this one opportunity to get this 100% right and thus avoid any further digging/disruption in the future.

We urge TII to extend the Foul Sewer from the Village Hall west to the Traffic Lights to accommodate all houses on the Rathkeale Road to Murphy's Cross, plus any future development on the southern/Adare Manor side of the traffic lights. The Foul Sewer also needs to be extended west beyond the lights to cater for future development on the council land at the Deerpark side.

Ducting for all other services required for those developments should be included to avoid further traffic chaos in the future and to minimise waste of taxpayers' money.

With thanks for your consideration,

Eileen Kennelly,
Secretary,
Adare Community Council,
St. Nicholas' House,
Adare,
Co. Limerick.
087 655 0044

Submission 9

Mid-West National Road Design Office,
Limerick City & Co.Council,
Lissanalta House,
Dooradoyle Road,
Limerick.
May 12th. 2016.

Submission on the N21 Adare Western Approach Pavement Improvement Scheme.

To Whom it May Concern.

I Tony Sexton and Family of Ardshanbally, Adare, are happy to see the above mentioned improvement works go ahead between Adare Village Hall and Murphy's Cross. We are well aware that it will impact on us all and traffic in all directions through Adare, but I think it will enhance the stretch involved immensely. I myself walk that stretch regularly and am very much aware of the dangers where no footpath exists.

Outside of the footpath completion to Murphy's Cross, the improvements will make the Boy's school area safer for children and parents dropping off/collecting, and also crossing the road to a new car park incorporating a holding tank. The renewal of the school fence which is crumbling and not safe or fit for purpose is also very welcome. The new footpath opposite the Boy's school plus road resurfacing will really enhance that area and help our Tidy Town's prospects.

All underground facilities should be put in place now for the future, and that must include **Foul Sewer pipes** to cater for the Rathkeale Road residents on septic tanks, developments on the Manor side near The Village/Manor Fields, and also the council land/sites at Deerpark side of that road. **Gas pipes and IT services ducting** should also be installed with the future in mind.

Good surface/soiled water ducting on both sides of the road to the holding tank is required.

Now is your chance to get it done right once and for all, on this very busy stretch of road. The finished road surface must also be well fit for purpose and be able to take the heavy traffic and avoid subsidence like previously.

Thank you and looking forward to a super job being done, that we and the National Road Design Staff will be proud of for years to come.

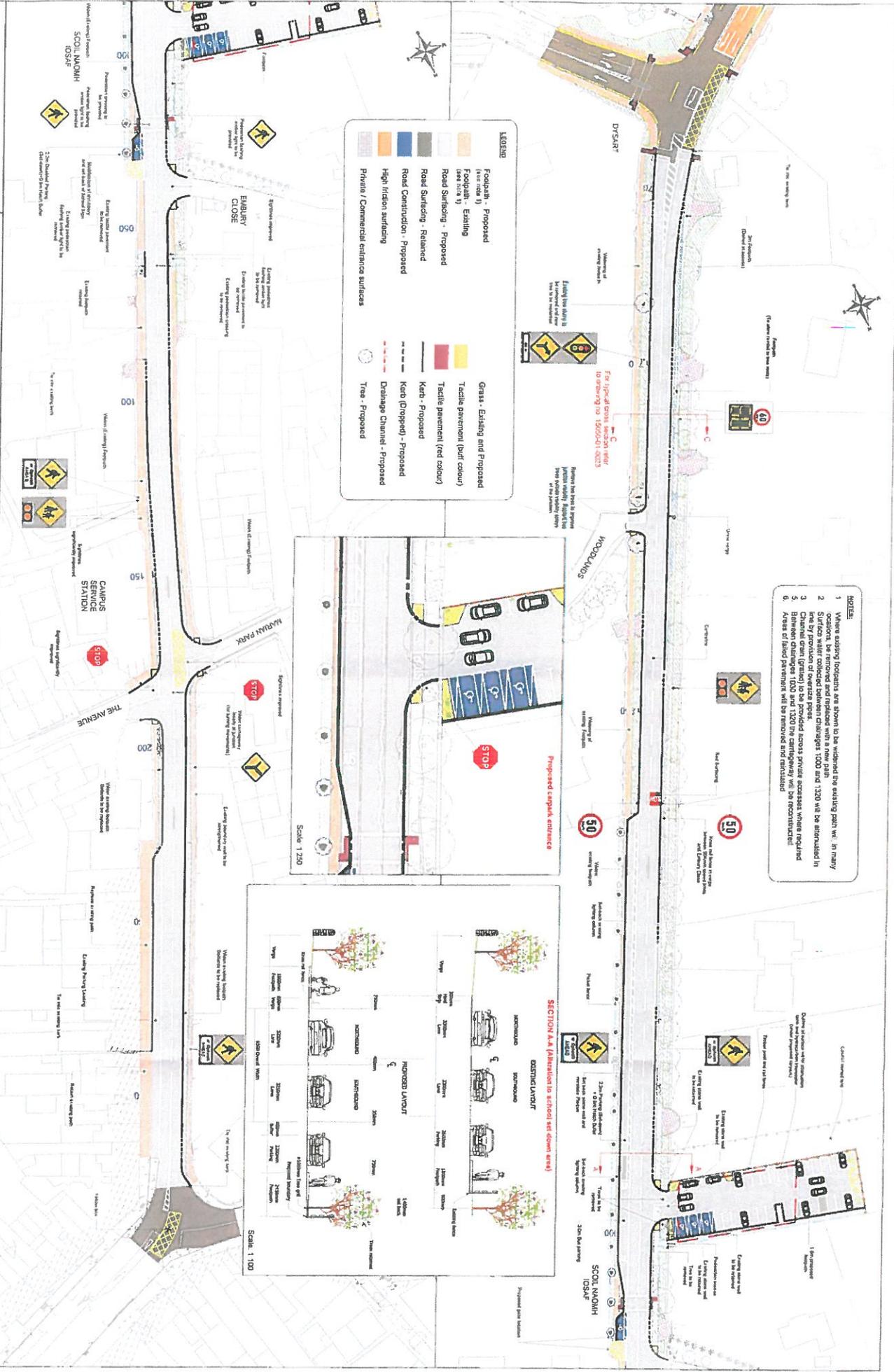
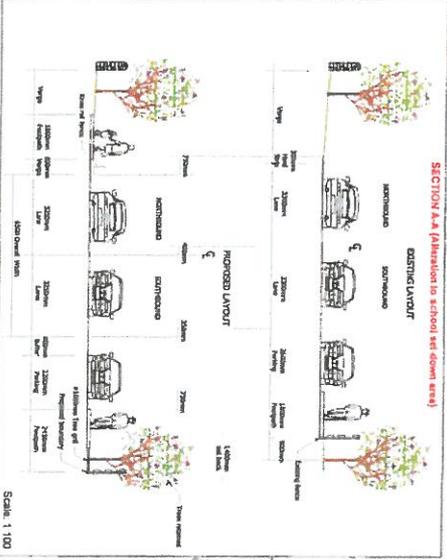
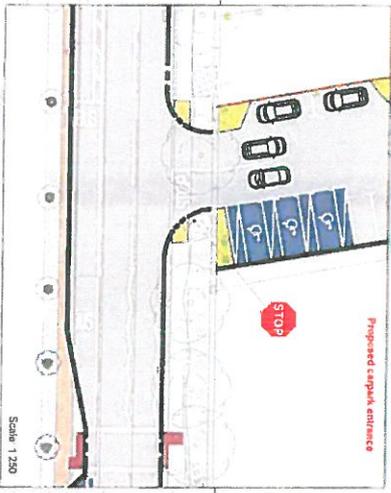
Yours sincerely,
Tony Sexton,
Ardshanbally, Adare, Co. Limerick.
Tel. 086/302 7816. Email: tonysextonadare@gmail.com.

APPENDIX 3
Site Layout Plans

- NOTES**
- 1 Where existing footpaths are shown to be widened the existing path will, in many locations, be removed and replaced with a new path.
 - 2 Surface water collected between changes 1000 and 1200 will be retained in channels (grass) to be provided across private accesses where required.
 - 3 Between changes 1000 and 1200 the carriageway will be reconstructed. Areas of failed pavement will be removed and replaced.

LEGEND

	Footpath - Proposed (see note 1)		Grass - Existing and Proposed (see note 1)
	Footpath - Existing (see note 1)		Tactile pavement (dark colour)
	Road Surfacing - Proposed		Tactile pavement (red colour)
	Road Surfacing - Relined		Kerb - Proposed
	Road Construction - Proposed		Kerb (Dropped) - Proposed
	High friction surfacing		Drainage Channel - Proposed
	Private / Commercial entrance surfaces		Tree - Proposed



ROADPLAN
993320226

7 Omorou Road,
Kilenny
Tel: +353(0)572350
e-mail: info@cadaplan.ie

Linerrick
Co. Limerick
Tel: +353(0)148000
e-mail: cadam@linerrick.ie

Mild West
National Road Design Office
Limerick City and
County Council
County Hall
Dorothy's
Dorothy's, Limerick
Tel: +353(0)148000
e-mail: info@roaddesign.ie

TIP
Transport Infrastructure Ireland
Parkgate Business Centre
Parkgate Street
Dublin 8
Tel: +353(0)16602511
e-mail: info@tip.ie

NO		DD									
Author	Checked	Author	Checked	Author	Checked	Author	Checked	Author	Checked	Author	Checked
<p>PROJECT: H27 Adare Western Approach Improvements & Signage</p> <p>Drawing Title: Site Layout Plan (Sheet 2 of 2)</p> <p>Scale: 1:500 Date: 17 May 18</p>											

